



DATE: March 1, 2006

TO: Chairman and Members of the Community Development Commission

FROM: Community Development Department/Planning Division

SUBJECT: **CONSIDERATION OF A RESOLUTION APPROVING REGULAR COASTAL PERMIT (RC-200-05) FOR THE CONSTRUCTION OF A PORTION OF A 44-MILE-LONG REGIONAL BICYCLE AND PEDESTRIAN PATHWAY/TRAIL LOCATED GENERALLY ALONG THE NORTH COUNTY TRANSIT DISTRICT RIGHT-OF-WAY. THE COMMUNITY DEVELOPMENT COMMISSION WILL TAKE ACTION ONLY ON THE PORTION BETWEEN WISCONSIN STREET AND NEPTUNE WAY – COASTAL RAIL TRAIL PHASE II – APPLICANT: CITY OF OCEANSIDE**

SYNOPSIS

The item under consideration is for a Regular Coastal Permit for the construction of a Coastal Rail Trail generally located along the south side of the railroad tracks from the Wisconsin Street parking lot moving north to Neptune Way. The proposed trail will integrate with the Phase I Coastal Rail Trail. Staff is recommending that the Commission approve the project and adopt the resolution as attached.

BACKGROUND

The Coastal Rail Trail is a proposed 44-mile-long regional bicycle and pedestrian pathway/trail that will extend from Oceanside to downtown San Diego. The Oceanside segment of the Coastal Rail Trail is the northernmost portion and would begin in northern Oceanside at the western terminus of the San Luis Rey Recreational Trail on the south side of the San Luis Rey River. The trail would continue south through Oceanside and would connect to the City of Carlsbad segment of the Coastal Rail Trail at Buena Vista Lagoon. The Carlsbad section of the trail is in the design phase. Further south, portions of the recreational trail have been built in Encinitas and are under construction in Solana Beach. The City of San Diego portion of the trail is in the preliminary design phase. The intent of the project is to generally follow the north-south coastal railroad tracks and right-of-way from central to northern San Diego County. The proposed project is identified in the City's Recreational Trails Sub-Element of the General Plan.

The Phase I trail from Vista Way to Morse Street on Broadway and Myers Streets is constructed and is now functioning as a Class I, II, and III trail. Class I bike paths are facilities with exclusive rights-of-way that are non-contiguous with the street with minimum or no cross flows by motorists. Where the trail is located in City streets, the street surface would be striped to designate the trail (Class II), or signs would be posted to designate the trail route (Class III). Phase II of the project (the current request) from Morse Street to Wisconsin Street will also consist of all three trail classifications.

Funding for the Coastal Rail Trail project is overseen by SANDAG. In 1999-2000, the Regional Transportation Improvement Program (RTIP) included \$5,824,000 in Congestion Mitigation and Air Quality (CMAQ) funding for the multi-jurisdictional Coastal Rail Trail Project. In addition, \$4,513,000 of Transportation Enhancement funds was programmed to support the Coastal Rail Trail between the City of Oceanside and the City of Solana Beach.

In Oceanside, initially \$425,844 was available for the design phase of the project. In January 2003, the SANDAG Transportation Committee allocated an additional amount of \$1,291,000 for the design and construction of the Coastal Rail Trail in Oceanside. This money will be used for design, processing and construction of Phase I of the bike trail and design and processing of Phase II of the project north to the Oceanside Transit Center.

On June 27, 2005, the Planning Commission approved that portion of the trail from Morse to Wisconsin streets.

Land Use and Zoning: The subject site is generally located within Subdistrict 14 although a portion of the trail (along Pacific and North Cleveland streets) is located within Subdistricts 5 and 12 of the "D" Downtown District. Subdistrict 14 is primarily intended to provide for public transportation and railroad uses. Subdistrict 5 is a high-density (29-43 dwelling units per acre) single and multifamily residential and Subdistrict 12 is primarily for a tourist-oriented subdistrict relating to the marina, pier and beach area.

Regular Coastal Permit: This project is also situated within the coastal zone and requires a Regular Coastal Permit. Under the provisions of the Local Coastal Plan the project site is designated as Transportation Utility, High-Density Residential and Coastal Dependent Recreational and Visitor Serving Commercial. The Transportation Utility Corridor classification serves as a link in the major passenger and freight line between Los Angeles and San Diego. The corridor includes open space, which buffers the railroad from surrounding noise – sensitive land uses and also serves as a reserve corridor for future transportation needs. The High Density Residential designation is intended to allow for high density multifamily or single-family residential and the Coastal Dependent Recreational and Visitor Serving Commercial is intended for specialized commercial uses directly dependant upon coastal uses.

In addition, a portion of the trail (along Pacific Street) is also situated within the “appeal jurisdiction” (the area east of the shoreline to the west side of Myers Street) of the local coastal zone. Under the certified Local Coastal Program (LCP), any local action by the City on this proposed coastal development permit might be appealed to the California Coastal Commission.

Project Description: The portion of the Phase II trail being considered by the Community Development Commission is generally located along the west side of the railroad tracks between Wisconsin and Tyson streets and along Pacific and North Cleveland Streets to Neptune Way. The trail enters the Redevelopment Project Area via the existing Wisconsin Street Parking Lot, continues north to Tyson Street, turns west two blocks to South Pacific Street, turns east to Pier View Way, turns north on Cleveland Street and finally turns west to Neptune Way to link up with the San Luis Rey Recreational Trail Phase I. Along the City's unimproved right-of-way (north of the Wisconsin Street Parking Lot) will be a Class I bike path and along the City streets will be a Class III bike path.

Environmental Determination: In accordance with the provisions of the California Environmental Quality Act, the Final Mitigated Negative Declaration (attached) has been prepared by the City's environmental planner. The document findings conclude that, with the application of the mitigation measures for raptors and tree replacement, the project would not have a significant adverse effect upon the environment.

ANALYSIS

Staff's review of the project has focused upon the project's consistency with the applicable goals of the Redevelopment Plan, Local Coastal Program, Recreational Trails Sub-Element of the General Plan, and the California Environmental Quality Act.

The project conforms with the Redevelopment Plan, City's Local Coastal Program and Recreational Trails Sub-Element by providing improved access for bikers and pedestrians to key beach areas. These are key goals of these plans. The project would further promote bicycle and pedestrian use and may encourage people to use alternative modes of transportation resulting in fewer vehicle trips. Between Wisconsin Avenue and Tyson Street, the edge of the bike path would be approximately 25 feet from the western railroad track. This distance of the path from the tracks in conjunction with provision of a six-foot-high fence between the path and tracks will ensure that the path can be used in a safe manner.

The environmental analysis for the project has determined that, with the implementation of mitigation measures, the project would not have a substantial adverse impact upon the environment. The full scope of specific mitigation measures has been attached to the project as recommended conditions of approval.

Staff also examined the proposed project consistency of the development with the policies of the Local Coastal Program. As designed, the project's visual impact would not significantly alter the appearance of the shoreline, since the project is generally located east of Myers Street and there are no proposed structures. In addition, the proposed project will not block public views from South Pacific Street. As such, the project would not be a visual impact to surrounding neighborhood or restrict public access or public coastal views. In addition, VI. Visual Resources and Special Communities, C. 2.e. of the Local Coastal Program "encourages continued development of bicycle and pedestrian facilities per the Local Coastal Program access policies."

In conclusion, staff believes that the project meets the intent of the Redevelopment Plan and the underlying subdistrict goals, which encourages the development of new alternative transportation uses. The project is also consistent with the land use policies and development criteria of the Local Coastal Plan.

COMMISSION OR COMMITTEE REPORTS

The Redevelopment Design Review Committee (RDRC) reviewed the project at its July 22, 2005 meeting. After extensive review and discussion of the project, the Committee approved the site design.

The Redevelopment Advisory Committee (RAC) will review the project at its February 27, 2006 meeting. Any recommendations or comments on the project will be presented to the Commission during the hearing for the project.

FISCAL IMPACT

Not applicable.

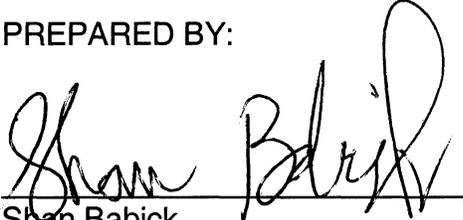
CITY ATTORNEY'S ANALYSIS

Pursuant to Oceanside Zoning Ordinance Article 4102, the Commission is authorized to hold a public hearing on this project's applications. Consideration of the project should be based on the evidence presented at the public hearing. After conducting the public hearing, the Commission shall approve, conditionally approve, or disapprove the project. The resolution has been reviewed and approved as to form by the City Attorney.

RECOMMENDATION

Staff recommends that the Commission adopt the resolution approving Regular Coastal Permit (RC-200-05) for the construction of Phase II of the Coastal Rail Trail generally located along the south side of the railroad tracks from Wisconsin Street parking lot to Neptune Way.

PREPARED BY:


Shan Babick
Associate Planner

SUBMITTED BY:


Steven R. Jepsen
Executive Director

SMBJH/vnm

REVIEWED BY:

Michelle Skaggs Lawrence, Assistant to the City Manager
Mike Blessing, Deputy City Manager
Frank Watanabe, Community Development Director
Jerry Hittleman, Interim City Planner





EXHIBITS/ATTACHMENTS

1. Resolution
2. Mitigated Negative Declaration
3. Site Plan

RESOLUTION NO.

A RESOLUTION OF THE COMMUNITY DEVELOPMENT COMMISSION OF THE CITY OF OCEANSIDE APPROVING A REGULAR COASTAL PERMIT FOR THE CONSTRUCTION OF A PORTION OF A 44-MILE LONG REGIONAL BICYCLE AND PEDESTRIAN PATHWAY/TRAIL LOCATED GENERALLY ALONG THE NORTH COUNTY TRANSIT DISTRICT RIGHT-OF-WAY. THE COMMUNITY DEVELOPMENT COMMISSION WILL ONLY TAKE ACTION ON THE PORTION OF TRAIL LOCATED BETWEEN WISCONSIN STREET AND NEPTUNE WAY WITHIN THE REDEVELOPMENT PROJECT AREA- APPLICANT: CITY OF OCEANSIDE

WHEREAS, on June 27, 2005, the Planning Commission held its duly noticed public hearing, and approved by a unanimous vote Regular Coastal Permit (RC-1-05) for the construction of a portion of a 44-mile long regional bicycle and pedestrian pathway/trail located generally along the North County Transit District right-of-way located between Morse Street and Wisconsin Street;

WHEREAS, on March 1, 2006, the Community Development Commission held its duly noticed public hearing, considered an application by the City of Oceanside for a Regular Coastal Permit (RC-200-05) for the construction of a portion of a 44-mile long regional bicycle and pedestrian pathway/trail located generally along the North County Transit District right-of-way located between Wisconsin Street and Neptune Way;

WHEREAS, the Redevelopment Design Review Committee (RDRC) of the City of Oceanside did, on July 22, 2005, review and recommend approval of the Regular Coastal Permit (RC-200-05);

WHEREAS, the Redevelopment Advisory Committee (RAC) of the City of Oceanside did, on February 27, 2006, review and recommend approval of Regular Coastal Permit (RC-200-05);

WHEREAS, the City expressly reserves the right to establish, modify or adjust any fee, dedication, reservation or other exaction to the extent permitted and as authorized by law;

WHEREAS, studies and investigations made by the Community Development Commission reveal the following facts:

FINDINGS:

For the Regular Coastal Permit:

1. The granting of the Regular Coastal Permit is consistent with the purposes of the California Coastal Act of 1976.
2. The proposed project is consistent with the policies of the Local Coastal Program as

1 implemented through the City Zoning Ordinance. In addition, the project will not substantially alter or
2 impact the existing coastal views through the public rights-of-way view corridors.

3 3. The proposed project will not obstruct any existing or planned public beach access;
4 therefore, the project is in conformance with the policies of Chapter 3 of the Coastal Act.

5 WHEREAS, pursuant to the California Environmental Quality Act of 1970, and State Guidelines
6 thereto; a Mitigated Negative Declaration has been prepared stating that if the mitigation measures are met
7 there will not be an adverse impact upon the environment.

8 WHEREAS, the Mitigated Negative Declaration together with any comments received, and
9 Mitigation and Monitoring and Reporting Program (MMRP) incorporated into the conditions of
10 approval for the project, were presented to the Community Development Commission, and the
11 Community Development Commission reviewed and considered the information contained in these
12 documents prior to making a decision on the project.

13 WHEREAS, the Mitigated Negative Declaration and Mitigation and Monitoring and Reporting
14 Program (MMRP) have been determined to be accurate and adequate documents, which reflect the
15 independent judgment and analysis of the Community Development Commission. On the basis of the
16 entire record before it, the Community Development Commission finds that there is no substantial
17 evidence that the project, with implementation of the mitigation measures proposed, will have a
18 significant impact on the environment.

19 WHEREAS, the documents or other material which constitute the record of proceedings upon
20 which the decision is based will be maintained by the City of Oceanside Planning Department, 300
21 North Coast Highway, Oceanside, California 92054.

22 WHEREAS, pursuant to Government Code §66020(d)(1), NOTICE IS FURTHER GIVEN that
23 the 90-day period to protest the imposition of any fee, dedication, reservation, or other exaction
24 described in this resolution begins on the effective date of this resolution and any such protest must be
25 in a manner that complies with Section 66020; and

26 WHEREAS, pursuant to Oceanside Zoning Ordinance §4603, this resolution becomes effective
27 upon its adoption.

28 NOW, THEREFORE, the Community Development Commission of the City of Oceanside does
29 resolve as follows:

SECTION 1. That Regular Coastal Permit (RC-200-05) and the Mitigated Negative

1 Declaration are hereby approved subject to the following conditions:

2 **Planning:**

3 1. This Regular Coastal Permit shall expire on March 1, 2008, unless implemented as
4 required by the Zoning Ordinance.

5 2. All of the conditions in Planning Commission Resolution No. 2005-P33 shall apply as
6 attached in Exhibit "A".

7 PASSED AND ADOPTED by the Oceanside Community Development Commission of the
8 City of Oceanside this ____ day of _____, 2006 by the following vote:

9 AYES:

10 NAYS:

11 ABSENT:

12 ABSTAIN:

13 _____
Chairman

14 ATTEST:

15 _____
16 Secretary

17 APPROVED AS TO FORM:
18 OFFICE OF THE CITY ATTORNEY

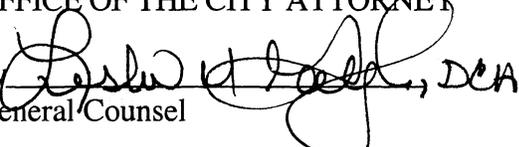
19 by  DCA
20 General Counsel

Exhibit "A"

PLANNING COMMISSION
RESOLUTION NO. 2005-P33

A RESOLUTION OF THE PLANNING COMMISSION OF THE
CITY OF OCEANSIDE, CALIFORNIA APPROVING A
REGULAR COASTAL PERMIT ON CERTAIN REAL
PROPERTY IN THE CITY OF OCEANSIDE

APPLICATION NO: RC-1-05
APPLICANT: City of Oceanside
LOCATION: Pacific Street, Oceanside Boulevard, North County Transit District
Right-of-Way

THE PLANNING COMMISSION OF THE CITY OF OCEANSIDE, CALIFORNIA DOES
RESOLVE AS FOLLOWS:

WHEREAS, there was filed with this Commission a verified petition on the forms prescribed by the Commission requesting a Regular Coastal Permit under the provisions of Articles 43 of the Zoning Ordinance of the City of Oceanside to permit the following:

construction of a recreational multi-use trail in City rights-of-way from Morse Street and Pacific Street to Wisconsin Avenue;
on certain real property described in the project description.

WHEREAS, the Planning Commission, after giving the required notice, did on the 13th day of June, 2005 conduct a duly advertised public hearing as prescribed by law to consider said application.

WHEREAS, pursuant to the California Environmental Quality Act of 1970, and State Guidelines thereto; a Final Mitigated Negative Declaration has been prepared for this project and states that, with the implementation of certain project conditions as mitigation measures, the project would not have any major significant adverse effect upon the environment;

WHEREAS, studies and investigations made by this Commission and in its behalf reveal the following facts:

FINDINGS:

For the Regular Coastal Permit:

1. The proposed project is consistent with the objectives and policies of the Local Coastal Program. Specifically, the project will facilitate and enhance the public's access and ability to enjoy coastal resources.

1 2. The proposed project, within the appeal area as identified in the Local Coastal Plan,
2 conforms to the public access and recreation policies of Chapter 3 of the Coastal Act.
3 The project is designed to improve and enhance accessibility to coastal areas in
4 Oceanside and other coastal cities to the south. The project will create new pedestrian
5 access ways and improve existing streets. The project will not obstruct any existing,
6 planned or required public beach access; therefore, the project is in conformance with
7 the policies of Chapter 3 of the Coastal Act.

8 NOW, THEREFORE, BE IT RESOLVED that the Planning Commission does hereby
9 approve the Final Mitigated Negative Declaration and approve Regular Coastal Permit (RC-1-05)
10 and subject to the following conditions:

11 **Engineering:**

- 12 1. The City's contractor shall monitor, supervise, and control all construction and
13 construction-supportive activities so as to prevent these activities from causing a public
14 nuisance and insuring strict adherence to the following points as inclusive, but not
15 necessarily limited to, construction responsibilities:
- 16 a) Dirt, debris, and other construction material shall not be deposited on any public
17 street or within the City's storm water conveyance system.
 - 18 b) All grading and related site preparation and construction activities shall be
19 limited to the hours of 7:00 a.m. to 6:00 p.m., Monday through Friday, and on
20 Saturday from 7:00 a.m. to 6:00 p.m. Hours for construction work may be
21 extended by the City Engineer for work that is not inherently noise-producing. All
22 work should utilize the latest technology for noise suppressive equipment.
23 Because construction noise may still be intrusive in the evening or on holidays,
24 the City of Oceanside Noise Ordinance also prohibits "any disturbing excessive,
25 or offensive noise which causes discomfort or annoyance to reasonable persons
26 of normal sensitivity."
 - 27 c) All construction staging areas shall be as far as possible from any existing
28 residential development. The construction site shall accommodate the parking of
29 all motor vehicles used by persons working at or providing deliveries to the site.
2. Sight distance requirements at all street intersections shall conform to the intersection sight

1 distance criteria as provided by the California Department of Transportation Highway
2 Design Manual.

3 3. A traffic control plan shall be submitted to and approved by the City Engineer prior to the
4 start of work within the City rights-of-way. Traffic control during construction adjacent to
5 or within all public streets must also meet Caltrans standards.

6 4. Any existing curb, gutter, pavement, or sidewalk any damaged or broken during
7 construction of the project shall be repaired or replaced as directed by the City Engineer.

8 5. This project shall provide year-round erosion control including measures for the site
9 required for the phasing of grading. Prior to the start of construction, an erosion control
10 plan, designed for all proposed stages of construction, shall be reviewed and approved by
11 the City Engineer.

12 6. A pre-construction meeting including the City's project manager, the City's contractor, the
13 City Public Works inspector, and City Environmental Planner shall be completed prior to
14 the start of any work for the approved project. The location of any construction staging
15 area, term of property use, and the implementation of the project conditions of approval
16 (including any and all conditions imposed by any agency having permit authority over the
17 project) as required by this Resolution, shall be confirmed at the pre-construction meeting.

18 7. Grading and drainage facilities shall be designed to adequately accommodate the local
19 storm water runoff and shall be in accordance with the City's Engineers Manual and as
20 directed by the City Engineer.

21 8. The project shall comply with all applicable regulations established by the United States
22 Environmental Protection Agency (USEPA) as set forth in the National Pollutant
23 Discharge Elimination System (N.P.D.E.S.) permit requirements for urban runoff and
24 storm water discharge and any regulations adopted by the City pursuant to the
25 N.P.D.E.S. regulations or requirements. The construction shall comply with all the
26 provisions of the N.P.D.E.S. and the Clean Water Program during and after all phases of
27 construction, including but not limited to: grading and street and landscaping
28 improvements. The City's contractor shall comply with all relevant regulations, when
29 they become effective, including but not limited to, incorporation into the design and
implementation of temporary structural Best Management Practices and non-structural

1 mitigation measures that would reduce pollution of storm water runoff to the maximum
2 extent practicable.

3 9. An erosion control plan and public improvement plan shall be prepared, reviewed, and
4 approved prior to the commencement of any construction activities. The plan shall reflect
5 all pavement, flatwork, landscaped areas, special surfaces, curbs, gutters, medians/stripping,
6 signage, the footprint of all utilities, any walls, drainage devices and the location utility
7 services.

8 10. Sediment, silt, grease, trash, debris, and/or pollutants shall be collected on-site and
9 disposed of in accordance with all state and federal requirements, prior to stormwater
10 discharge either off-site or into the City drainage system.

11 Planning and Environmental Impact Mitigation:

12 11. This Regular Coastal Permit shall expire on June 27, 2007 unless implemented as required
13 by the Zoning Ordinance.

14 12. This Regular Coastal Permit approves only the following: construction of a multi-use
15 recreational trail between Morse Street and Pacific Street and Wisconsin Avenue. The
16 scope of the approved project is shown on the plans and exhibits presented to the Planning
17 Commission for review and approval. No deviation from the approved project and the
18 approved project plans and exhibits shall occur without Planning Department approval.
19 Substantial deviations shall require a revision to the Regular Coastal Permit or a new
20 Regular Coastal Permit.

21 13. Landscape plans, meeting the criteria of the City's Landscape Guidelines and Water
22 Conservation Ordinance No. 91-15, including the maintenance of such landscaping, shall
23 be reviewed and approved by the City Engineer and Planning Director prior to the issuance
24 of construction permits. Landscaping shall not be installed until the improvements plans
25 have been finally approved.

26 14. An operations and management plan for any temporary construction staging area shall be
27 reviewed and approved by the Harbor Master prior to its occupancy and use by the City's
28 contractor.

29 15. The City's project manager shall ensure that the City's contractor is provided with copies
of the staff report, resolution(s) of approval, and environmental documentation for the

1 project. The contractor shall be responsible to implement and adhere to the requirements
2 of the project approval, in as much as the scope and agreement of his contract with the City
3 requires.

4 16. Failure to meet any conditions of approval for this development shall constitute a violation
5 of the Regular Coastal Permit.

6 17. Unless expressly waived, all current zoning standards and City ordinances and policies
7 in effect at the time of issuance of any construction permits are required to be met by this
8 project.

9 18. An erosion control plan shall be implemented in conformance with the City of Oceanside
10 erosion control ordinance.

11 19. Material and waste Best Management Practices (BMPs) during site grading and
12 construction such as solid, sanitary, hazardous, contaminated soil, concrete construction
13 and construction waste management, spill prevention, appropriate material delivery and
14 storage, employee training, dust control, and equipment cleaning, maintenance and fueling
15 shall be strictly enforced.

16 20. A pre-construction survey for potential nesting raptor species shall be performed at least
17 one week prior to construction. If all trees were removed at one time, additional raptor
18 surveys would be required. However, if any trees remain, raptor surveys shall be
19 performed once a month during raptor breeding season (February 1 to August 31) until
20 construction is completed. If nesting raptors are observed in the planned trail area,
21 construction shall be discontinued in the vicinity of the nest while it is active.

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29

1 21. The trees used for path construction shall be replaced at a 1:1 ratio somewhere within the
2 path alignment.

3 PASSED AND ADOPTED Resolution No. 2005-P33 on June 27, 2005 by the following
4 vote, to wit:

5 AYES: Barrante, Chadwick, Parker, Schaffer, Neal and Nack

6 NAYS: None

7 ABSENT: Todd

8 ABSTAIN: None

9
10 
11 _____
George Barrante, Chairman
Oceanside Planning Commission

13 ATTEST:

14 
15 _____
16 Gerald S. Gilbert, Secretary

17
18 I, GERALD S. GILBERT, Secretary of the Oceanside Planning Commission, hereby certify that
19 this is a true and correct copy of Resolution No. 2005-P33.

20 Dated: June 27, 2005



CITY OF OCEANSIDE
PLANNING DEPARTMENT

MITIGATED NEGATIVE DECLARATION

TO: OFFICE OF PLANNING AND RESEARCH
1400 TENTH STREET, ROOM 121
SACRAMENTO, CA 95814

RECORDER/COUNTY CLERK
COUNTY OF SAN DIEGO
P.O. BOX 1750
SAN DIEGO, CA 92112-4147

PROJECT TITLE AND FILE NUMBER:
COASTAL RAIL TRAIL PHASE II PROJECT

PROJECT LOCATION:
Between Oceanside Boulevard and Pier View Way along railroad and street right-of-way.

PROJECT DESCRIPTION:

The proposed project to construct the northernmost portion of the Coastal Rail Trail in the City of Oceanside. The proposed trail would include a Class I bike trail constructed adjacent to the west side of the North County Transit District (NCTD) railroad tracks within City and NCTD right-of-way between Tyson Street and Oceanside Boulevard. - The remainder of the trail will be on the in street right-of-way on Pier View Way, Pacific Street and Cleveland Street.

FINDING: Pursuant to the provisions of Ordinance No. 88-31, pertaining to procedures and guidelines to implement the California Environmental Quality Act (Public Resources Code Section 21000 et. al.), the proposed project has been reviewed by the Environmental Review Committee established by ordinance to be responsible for evaluating the information. The Environmental Review Committee, after study of the facts and findings, has on March 16, 2005 determined that the project will not have a significant effect on the environment.

THE PROJECT WAS DETERMINED TO HAVE NO MAJOR SIGNIFICANT ADVERSE EFFECT UPON THE ENVIRONMENT.

THE PROJECT WAS DETERMINED TO HAVE NO MAJOR SIGNIFICANT ADVERSE EFFECTS UPON THE ENVIRONMENT PER COMPLIANCE WITH THE FOLLOWING CONDITIONS:

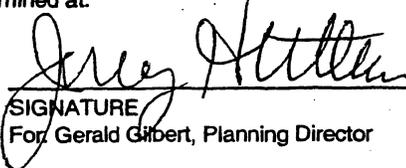
See attached initial study

Initial Study prepared by:
Jerry Hittleman, Senior Planner

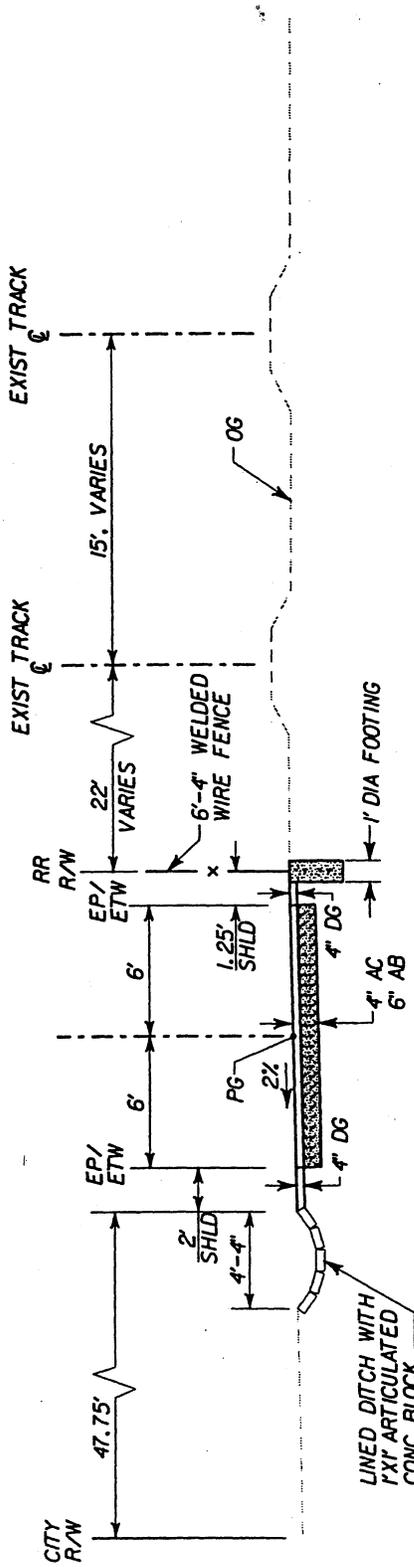
Contact Person:
Brian Grove, Project Manager

The Initial Study is available for public review and may be examined at:

City of Oceanside
Planning Department
300 N. Coast Highway
Oceanside, CA 92054

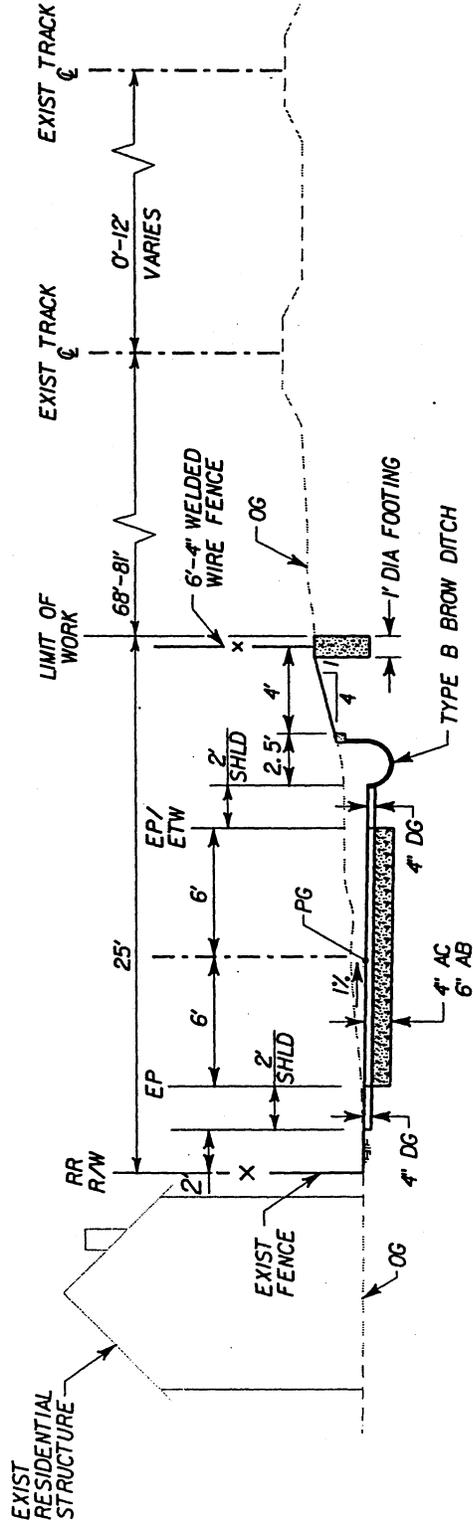

SIGNATURE
For Gerald Gilbert, Planning Director

March 16, 2005
DATE



TYPICAL SECTION

COASTAL RAIL TRAIL
 GENERALLY WISCONSIN AVE TO TYSON ST
 NOT TO SCALE



TYPICAL SECTION

COASTAL RAIL TRAIL
 GENERALLY OCEANSIDE BLVD TO WISCONSIN AVE
 NOT TO SCALE

FIGURE 4

DE DOKKEN
 ENGINEERING

955 Chesapeake Dr., Suite 405 San Diego, CA 92123 650.54-3377

COASTAL RAIL TRAIL
 PHASE 2
 CITY OF OCEANSIDE
 PROJECT LIMITS
 TYPICAL SECTIONS
 FEB 11, 2006

**FINAL
MITIGATED NEGATIVE DECLARATION
COASTAL RAIL TRAIL PROJECT
PHASE 2
CITY OF OCEANSIDE**

RECEIVED
DEC 23 2005
Planning Department



Prepared for:

City of Oceanside
300 N. Coast Highway
Oceanside, CA 92054-2885

Prepared by:

EDAW, Inc.
1420 Kettner Boulevard, Suite 620
San Diego, California 92101

for:

Dokken Engineering
9665 Chesapeake Drive, Suite 435
San Diego, California 92123-1267

December 2005

Preface

This is a Final Mitigated Negative Declaration (MND), prepared pursuant to the California Environmental Quality Act (CEQA), addressing potential environmental consequences of the construction of the northern segment of the Coastal Rail Trail bicycle and pedestrian path in the City of Oceanside. The Draft MND was circulated for public review for a 30 day period that concluded on April 15, 2005. Two agencies provided comments: the North County Transit District and the Sempra Energy. Both comment letters and responses to those letters are provided following this preface. The MND was provided to the State Clearinghouse, but no state agencies submitted comments. A letter from the Clearinghouse is attached as well.

After conclusion of the public review period, minor modifications were made to the project design, which involved realignment of a portion of the trail between approximately Oak Street and Tyson Street. The purpose of this minor shift in trail alignment was to move the trail farther away from the railroad tracks. This realigned portion of the trail was set back approximately 30 feet west of its previous alignment directly adjacent to the North County Transit District (NCTD) right-of-way (ROW) and relocated within the center of City-owned property, between the ROW and the residences on Meyers Street (shown in Sheet 5 and Sheet 6 of 9). The realigned trail is now 30 feet closer to the residences on Meyers Street, but is separated from the homes by approximately 30 feet. No other trail alignment modifications were required. No new impacts were identified as a result of the trail realignment; therefore no modifications to the Final MND were required.

Funding for this project comes in part from federal funds released by the Federal Highways Administration (FHWA) and must be evaluated per the National Environmental Policy Act (NEPA). Caltrans, District 11 is the local agency coordinating with FHWA for compliance with NEPA and other federal requirements. Accordingly, this MND contains technical reports and documents prepared to satisfy Caltrans requirements. The cultural resources evaluation to satisfy Section 106 compliance is contained in the Archaeological Survey Report (ASR) and Historic Property Survey Report (HPSR) – both of which are negative. The Coastal Consistency Determination (CCD) is provided as Attachment D.



April 22, 2005

RECEIVED

APR 25 2005

Planning Department

Mr. Jerry Hittleman
City Oceanside
Planning Department
300 North Coast Highway
Oceanside, California 92054

SUBJECT: NORTH COUNTY TRANSIT DISTRICT (NCTD) COMMENTS ON THE DRAFT MITIGATED NEGATIVE DECLARATION FOR THE COASTAL RAIL TRAIL PROJECT-PHASE 2 IN THE CITY OF OCEANSIDE

Dear Mr. Hittleman:

Thank you for providing NCTD with the opportunity to comment on the Draft Mitigated Negative Declaration (MND) for the Coastal Rail Trail Project – Phase 2 in the City of Oceanside, dated February 2005. NCTD values the opportunity to work cooperatively with the City on this Project and offers the following comments regarding the MND.

As portions of the proposed Project will be located adjacent to or within the NCTD railroad right-of-way, NCTD has concerns of potential hydrology and water quality impacts the proposed Project may have to the railroad right-of-way. Review of the MND document indicates that the proposed bike path would result in an increased area of impervious surface; and hence, generate a minimal increase in surface runoff. NCTD is aware that the City has taken an active approach to storm water regulation and we trust that the City will incorporate its water management strategies into this Project. As with Phase 1 of the bike path, we anticipate the City will be designing and implementing a Project Storm Water Pollution Prevention Plan (SWPPP) for Phase 2, which we request an opportunity to review.

In addition, and as emphasized by FHWA, NCTD would like to highlight that it is our understanding the proposed Project is for uses associated with providing a multi-modal, non-motorized trail for transportation and incidental recreational activities.

Thank you again for providing NCTD with the opportunity to comment on the Draft Mitigated Negative Declaration (MND) for the Coastal Rail Trail Project - Phase 2 in the City of Oceanside. NCTD looks forward to continued communication and cooperation

Jerry Hittleman
Page 2
April 22, 2005

with the City of Oceanside on this Project. Please feel free to contact me at (760) 967-2817 should you have any questions regarding the above NCTD comments.

Sincerely,

A handwritten signature in cursive script that reads "Kate Stonelake".

Kate Stonelake
Rail Project & Environmental Coordinator, NCTD

cc: Richard Walker, NCTD
Jim Cunningham, NCTD
Mitch Alderman, NCTD
Ed Singer, NCTD

Response to Kate Stonelake, North County Transit District

A Storm Water Pollution Prevention Plan is being prepared for Phase I of this project. The plan will be available for review by the NCTD, as requested. As required by the City, a Standard Urban Stormwater Mitigation Plan is being prepared to document the effectiveness of project drainage/run-off control features.

Based on the type and limited duration of construction activities, the project design features intended to allow water infiltration and sediment retention, and the use of best management practices during the construction phase of the project, impacts to surface water quality from construction runoff will be minimal. The proposed bike path will result in an increased area of impervious surface approximately 0.23 hectare (0.57 acre) in size, generating a minimal increase in surface water runoff. The project will adhere to any waste discharge requirements and water quality standards.

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April 28, 2005

Mr. Jerry Hittleman
City of Oceanside
Planning Department
300 North Coast Highway
Oceanside CA 92054

**RE: MITIGATED NEGATIVE DECLARATION FOR THE COASTAL RAIL
TRAIL PHASE II PROJECT**

Mr. Hittleman:

San Diego Gas and Electric Company (SDG&E) would like to thank the City of Oceanside for the opportunity to review the Mitigated Negative Declaration (MND) for the Coastal Trail Phase II Project. A review of our facilities maps indicates that SDG&E has both natural gas and electrical facilities within the project area.

As described in the MND, the proposed Class III bike routes along Pacific Street (south and north), Oceanside Boulevard, Tyson Street, Pier View Way and Cleveland Street do not require modification of existing roadway or sidewalk paving within public street rights-of-way. As described, the proposed Class III bike routes do not appear to have the potential to affect SDG&E's existing underground natural gas facilities or overhead electric transmission and distribution facilities in the project area.

As described in the MND, the Class I bike paths improvements within railroad rights-of-way from Oceanside Boulevard north to Tyson Street may require some ground disturbance (grading, excavating) to install curbs, paving and fencing during construction. Our records indicate:

- A 1-1/2 inch high pressure natural gas line within a three-inch steel casing at the Class I bike path crossing on Oceanside Boulevard
- A 3-inch natural gas line at the Class I bike path crossing on Oak Street

We suggest that a Dig-Alert verification for these underground natural gas facilities be performed at these street crossings prior to any excavation or grading required for the construction of the Class I bike paths.

As the project proceeds, the City should notify SDG&E of any changes to the project description, scope of work or field conditions from that described in the MND requiring work affecting SDG&E gas or electrical facilities in the project area.

Please call me at (858) 637-3709 if you have any questions.

Sincerely



Mark Chomyn
Land Planner
SDG&E

Cc: Jerry Pinney, SDG&E Sr. Field Utility Specialist
Alan Dusi, SDG&E Sr. Engineer Gas Transmission
Mike Williams, SDG&E Land Management

Response to Mark Chomyn, San Diego Gas & Electric

The City will coordinate with SDG&E to properly identify any natural gas and electrical facilities in the project area. A Dig-Alert verification will be performed prior to any ground disturbance required for the construction of the project.

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STATE OF CALIFORNIA
 Governor's Office of Planning and Research
 State Clearinghouse and Planning Unit



Arnold
 Schwarzenegger
 Governor

Sean Walsh
 Director

April 18, 2005

RECEIVED

APR 25 2005

Planning Department

Jerry Hittleman
 City of Oceanside
 300 North Coast Highway
 Oceanside, CA 92054

Subject: Coast Rail Trail Phase II
 SCH#: 2005031089

Dear Jerry Hittleman:

The State Clearinghouse submitted the above named Negative Declaration to selected state agencies for review. The review period closed on April 15, 2005, and no state agencies submitted comments by that date. This letter acknowledges that you have complied with the State Clearinghouse review requirements for draft environmental documents, pursuant to the California Environmental Quality Act.

Please call the State Clearinghouse at (916) 445-0613 if you have any questions regarding the environmental review process. If you have a question about the above-named project, please refer to the ten-digit State Clearinghouse number when contacting this office.

Sincerely,

Terry Roberts
 Director, State Clearinghouse

**Document Details Report
State Clearinghouse Data Base**

SCH# 2005031089
Project Title Coast Rail Trail Phase II
Lead Agency Oceanside, City of

Type Neg Negative Declaration
Description Construction of a Class I and Class III bike/pedestrian trail on street and railroad right-of-way.

Lead Agency Contact

Name Jerry Hittleman
Agency City of Oceanside
Phone (760) 435-3535 **Fax**
email
Address 300 North Coast Highway
City Oceanside **State** CA **Zip** 92054

Project Location

County San Diego
City Oceanside
Region
Cross Streets Cleveland Street / Pacific Street
Parcel No.
Township **Range** **Section** **Base**

Proximity to:

Highways 76
Airports
Railways San Diego Northern
Waterways Pacific Ocean
Schools
Land Use Railroad and Street Right-of-Way / Public Service and Redevelopment

Project Issues Coastal Zone; Drainage/Absorption; Recreation/Parks; Traffic/Circulation; Vegetation

Reviewing Agencies Resources Agency; Regional Water Quality Control Board, Region 9; Department of Parks and Recreation; Native American Heritage Commission; Department of Fish and Game, Region 5; Department of Water Resources; California Highway Patrol; Caltrans, District 11; Air Resources Board, Transportation Projects; State Lands Commission; California Coastal Commission

Date Received 03/17/2005 **Start of Review** 03/17/2005 **End of Review** 04/15/2005

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| B Archaeological Survey Report, Historic Property Survey Report (both Negative) (<i>Bound Separately</i>) | |
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INTRODUCTION

The City of Oceanside (City) proposes to construct a segment of the Coastal Rail Trail, a 71-kilometer-long (44-mile-long) regional bicycle and pedestrian pathway/trail that will extend from Oceanside to downtown San Diego. The Oceanside segment of the Coastal Rail Trail is the northernmost portion and will begin in northern Oceanside at the western end of the San Luis Rey Recreational Trail. The trail will continue south through the City and connect to the City of Carlsbad segment of the Coastal Rail Trail. Approximately 5 kilometers (3 miles) of the Coastal Rail Trail will be located in Oceanside. The trail will be located within the North County Transit District (NCTD) railroad right-of-way, City right-of-way, or on City streets. Where the trail will be in or adjacent to railroad right-of-way, the trail will be constructed as a Class I bike path. Class I bike paths are facilities with exclusive rights-of-way, with minimum cross flows by motorists. Where the trail will be located on City streets, signs will be posted to designate the trail route (Class III).

The alignment of the Coastal Rail Trail in Oceanside was evaluated in the approved *Final Mitigated Negative Declaration* (MND), *Coastal Rail Trail Project, Oceanside to Del Mar* (City of Carlsbad 2001). Subsequently, two minor modifications to the Oceanside alignment have been made. The southern modification is located generally between South Myers Street and Vista Way. This has been evaluated in a separate MND and has been constructed. The subject of this MND is a modification to the northern segment, generally between the San Luis Rey trail connection and Oceanside Boulevard. In the 2001 Final MND, the segment between Tyson Street and Oceanside Boulevard consisted of a Class I bike path on the east side of the NCTD railroad tracks. NCTD now proposes to build an additional railroad track in this location, so the bike path is proposed to be relocated to the west side. To route bicycle traffic to the west side, a portion of the Class III bike route along Cleveland Street, Neptune Way, Pier View Way and Pacific Street will be modified as well. More detailed information about the original project and modifications are provided in the Project Description (Section 8) of the Initial Study Checklist.

As part of this environmental review process, technical specialists reviewed the alignment for biological resources, cultural resources, and known hazardous materials. The results of the cultural resource and hazardous materials studies were negative. The biological survey identified trees which may offer nesting habitat for raptors and identified a process to minimize impacts if construction occurs during nesting. The three studies are attached to this MND/IS and a more detailed summary of the studies is provided in the Project Description portion (Section 8) of the Initial Study Checklist.

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ENVIRONMENTAL CHECKLIST

1. Project title:

Coastal Rail Trail, City of Oceanside, Phase 2

2. Lead agency name and address:

City of Oceanside Planning Department
300 N. Coast Highway
Oceanside, CA 92054-2885

3. Contact person and phone number:

Jerry Hittleman, City of Oceanside
(760) 435-3535 (phone)
(760) 754-2958 (fax)

4. Project location:

The Oceanside segment of the Coastal Rail Trail is the northernmost portion and will begin in northern Oceanside at the western end of the San Luis Rey Recreational Trail. The trail will continue south through the City and connect to the Carlsbad segment of the Coastal Rail Trail. Approximately 5 kilometers (3 miles) of the Coastal Rail Trail will be located in the City. The trail will be located within the North County Transit District (NCTD) railroad right-of-way, City right-of-way, or on City streets.

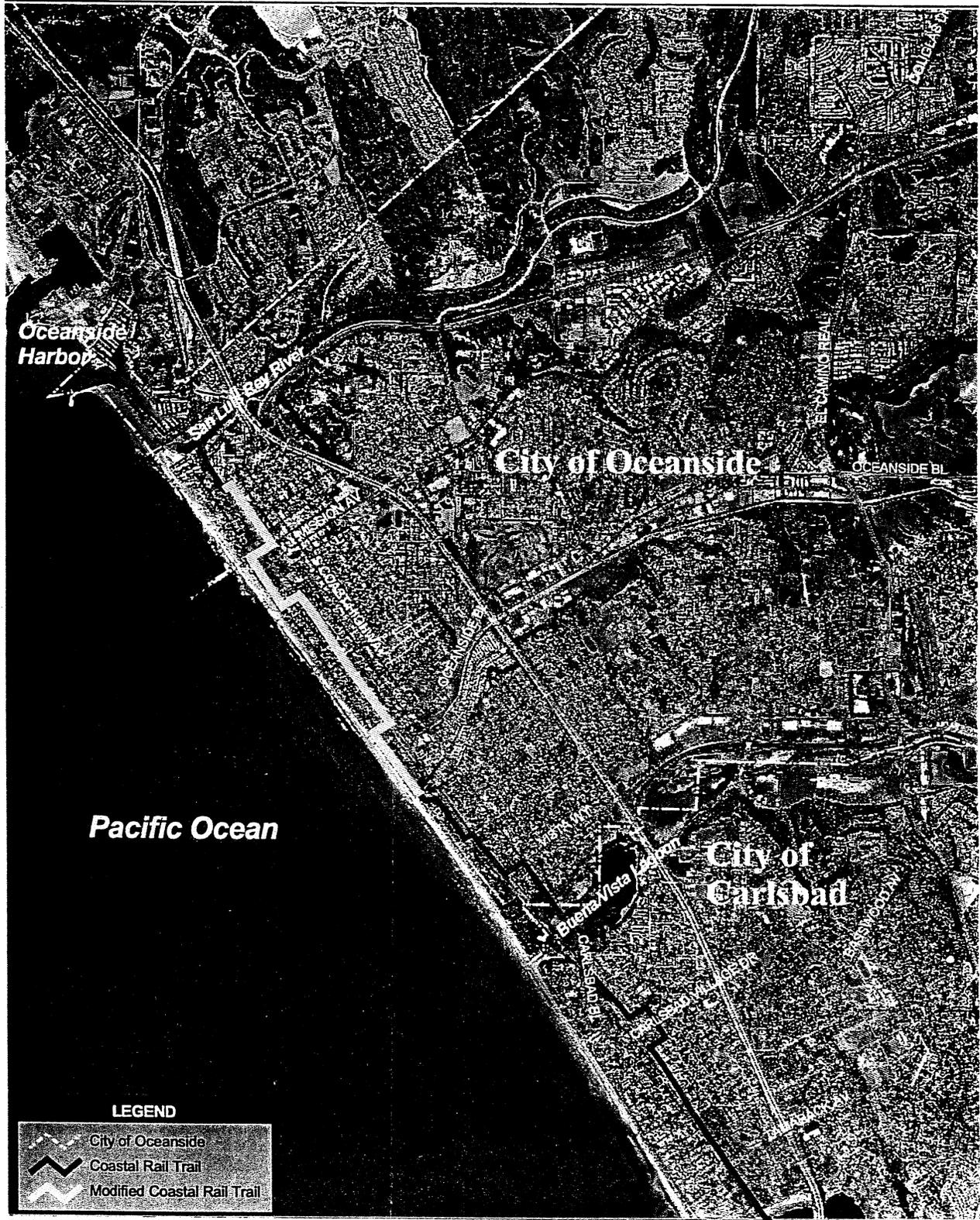
The portion of the trail that is the subject of this MND will begin in northern Oceanside at Pier View Way and Cleveland Street and travel west on Pier View Way, south on Pacific Street, and east on Tyson Street as a Class III bike route. A Class I bike path will be constructed adjacent to the west side of the NCTD railroad tracks between Tyson Street and Oceanside Boulevard, within City and NCTD rights-of-way (Figures 1 and 2).

5. Project sponsor's name and address:

Brian Grove
City of Oceanside Public Works Department
300 N. Coast Highway
Oceanside, CA 92054-2885
(760) 435-5106 (phone)

6. General plan designation:

Open Space and Redevelopment



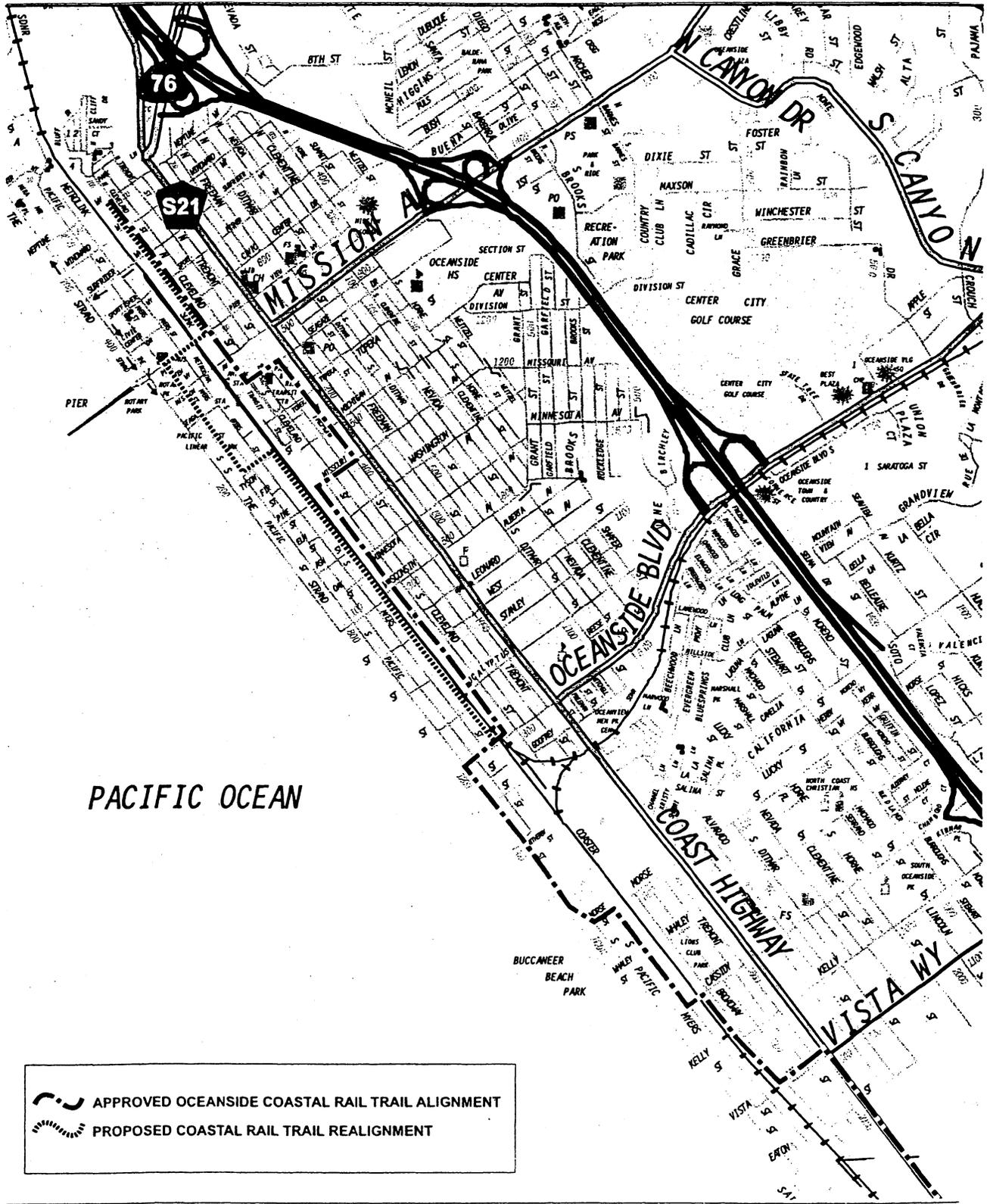
Source: SANDAG, 2002 (Base Layers); AirPhotoUSA, 2001.



4000 0 4000 Feet

Scale: 1 : 48 000; 1 inch = 4000 feet

Figure 1
Regional Map



Source: Thomas Bros. 2002

Figure 2
Vicinity Map

Oceanside Coastal Rail Trail

1K219 Coastal Rail Trail/Figures/ Fig 2 Vicinity Map04 (brady) 1/17/03

7. Zoning:

Public/Semi Public Space and “D” Downtown District-Subdistrict 14

8. Description of project:

The proposed project provides connection between two existing bike paths in Oceanside. The San Luis Rey River trail runs generally east-west on the south side of the river and the southern portion of the Coastal Rail Trail has been implemented generally from Buccaneer Park south to the Oceanside city limit. With its implementation, the project will provide for a signed bike route generally north-south through the entire city of Oceanside. As noted in the Introduction, the Phase 2 project (the subject of this MND) is a modification of the CRT previously approved for implementation in Oceanside. More information about the previously approved project and proposed bike facility is provided below. This is followed by details regarding the Phase 2 project, as well as an overview of the three key environmental resource evaluations undertaken for this MND. These three studies address biological resources, cultural resources, and hazardous materials.

Comparison With Previously Approved Route

In the alignment analyzed in the Final MND, a Class III bike route was to head south from the San Luis Rey Recreational Trail, east on Neptune Way, south on Cleveland Street, east on Seagaze Way, south on Tremont Street, and west on Missouri Avenue. From Missouri Avenue to Oceanside Boulevard, the trail would have continued south as a Class I bike path within the railroad right-of-way on the east side of the railroad tracks. A Class III bike route would have continued west on Oceanside Boulevard and south on Pacific Street. The bike route would have transitioned to a Class I bike path at Buccaneer Park, utilizing an existing pedestrian path, and then run along the east side of the railroad right-of-way to Vista Way. At Vista Way, the trail would transition to a Class III route to Coast Highway Class II bicycle lanes.

Two segments of the Oceanside Coastal Rail Trail in Oceanside will be different from that described in the 2001 Final MND. One segment is the portion between Buccaneer Park and the Oceanside city limit. That segment has been redesigned to remain on streets and outside of the railroad right-of-way. It is now located on Morse Street between Pacific Street and South Myers Street, South Myers Street between Morse Street and Cassidy Street, and Broadway between Cassidy Street and Vista Way. Construction has been completed on this segment.

The second segment that differs from that described in the 2001 Final MND, and the subject of this MND, is a proposed Class I bike path on the west side of the NCTD railroad tracks between Tyson Street and Oceanside Boulevard, within city and NCTD rights-of-way. Originally, this segment of the trail was proposed to be constructed on the east side of the railroad tracks near the transit station. NCTD now has plans to add a railroad track on the east side of the existing one, therefore eliminating the potential use of that side for the bike trail. In response, the City has proposed to realign that portion of the Coastal Rail Trail to the west side of the tracks. To facilitate the western relocation, the connection to the north must be modified slightly. Instead of going around the Oceanside Transit Station on the east side via Tremont Street, the bike route will head west and go around the transit station via Pacific Street (see Figure 3, Sheets 1 through 9).

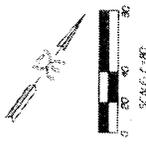
NOTE:
 NO FURNISHMENT IMPROVEMENTS OR ROWWAY STRIPING IS PROPOSED ALONG THE CLASS III BIKE ROUTE.
 SIGNS TO DIRECT BIKE ROUTE WILL BE MOUNTED ON EXISTING LIGHT POLES OR SIGN POSTS.



MATCH LINE-SEE SHEET 2

Martin D. Rosen
 Martin D. Rosen
 Senior Environmental Planner
 Caltrans District 11 POS
 Date

Gary Vafese
 Gary Vafese
 District Local Assistance Engineer
 Caltrans District 11 Project Manager
 Date 4/29/05



DEDOKKEN
 SCULPTURAL DESIGN
 WWW.DEDOKKENDESIGN.COM
 945 Decoyote Drive, Suite 415 San Diego, CA 92123 (619) 544-8377

PROJECT: COASTAL RAIL TRAIL
 PHASE 2
 CITY OF OCEANSIDE
 APE LIMITS
 SHEET 1 OF 8
 APRIL 28, 2005

MATCH LINE-SEE SHEET 1



OCEANSIDE BLVD

CLASS I BIKE PATH ALIGNMENT
CLASS III BIKE ROUTE

LEGEND

AERIAL FLOWN SPRING 2000

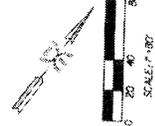
MATCH LINE -
SEE SHEET 3

DE DOKKEN
ENGINEERING

PROJECT: COASTAL RAIL TRAIL
PHASE 2
CITY OF OCEANSIDE

APE LIMITS
SHEET 2 OF 8
APRIL 28, 2005

3605 Crosswath Drive, Suite 415 San Diego, CA 92121 619-594-8377



NOTE:
NO PERMANENT IMPROVEMENTS OR ROADWAY STOPPING IS PROPOSED ALONG THE CLASS II BIKE ROUTE.
SIGNS TO DIRECT BIKE ROUTE WILL BE MOUNTED ON EXISTING LIGHT POLES OR SIGN POSTS.



MATCH LINE -
SEE SHEET 2

MATCH LINE-SEE SHEET 4

OCEANSIDE BLVD

S MEYERS ST

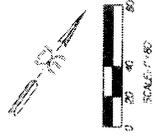
FORSTER ST

LANDSCAPED MEDIAN
WITH LIGHTING AND
SAFETY SIGNS
CONSTRUCTED AT BIKE
PATH TERMINALS

AERIAL FLOWN SPRING 2000

LEGEND

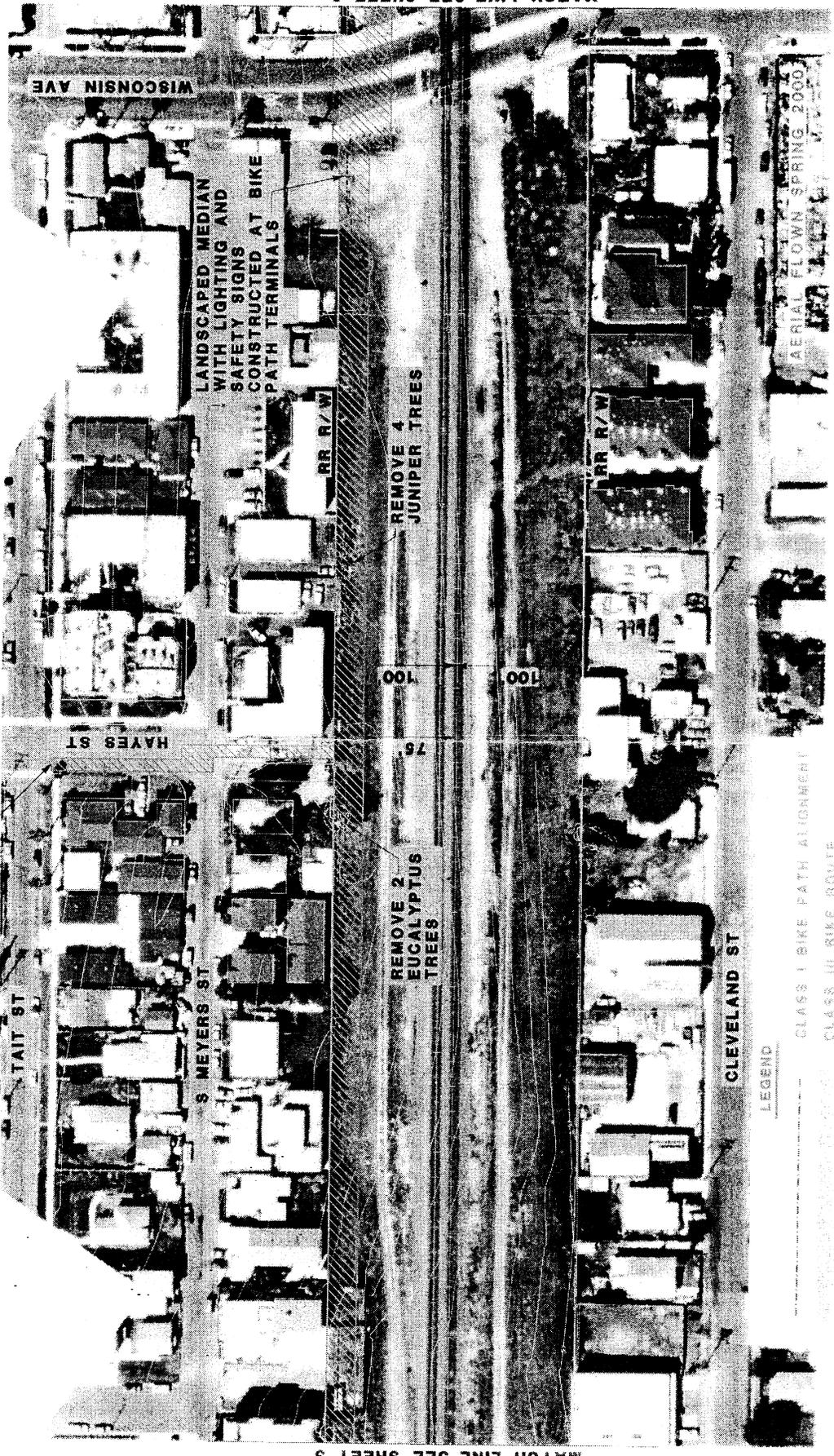
- CLASS I BIKE PATH ALIGNMENT
- CLASS III BIKE ROUTE
- PERMANENT IMPROVEMENTS



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PROJECT: COASTAL RAIL TRAIL
PHASE 2
CITY OF OCEANSIDE
APE LIMITS
SHEET 3 OF 8
APRIL 28, 2005

18" CONCRETE PIPE PLACED
IN STREET, CONNECT TO
EXISTING STORM DRAIN
AT PACIFIC STREET



WISCONSIN AVE

HAYES ST

S MEYERS ST

CLEVELAND ST

MATCH LINE-SEE SHEET 3

MATCH LINE-SEE SHEET 5



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PHASE 2
CITY OF OCEANSIDE
APE LIMITS
SHEET 4 OF 9
APRIL 28, 2005

LEGEND
CLASS I BIKE PATH ALIGNMENT
CLASS III BIKE ROUTE
PERMANENT IMPROVEMENTS

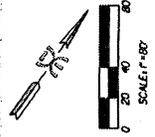
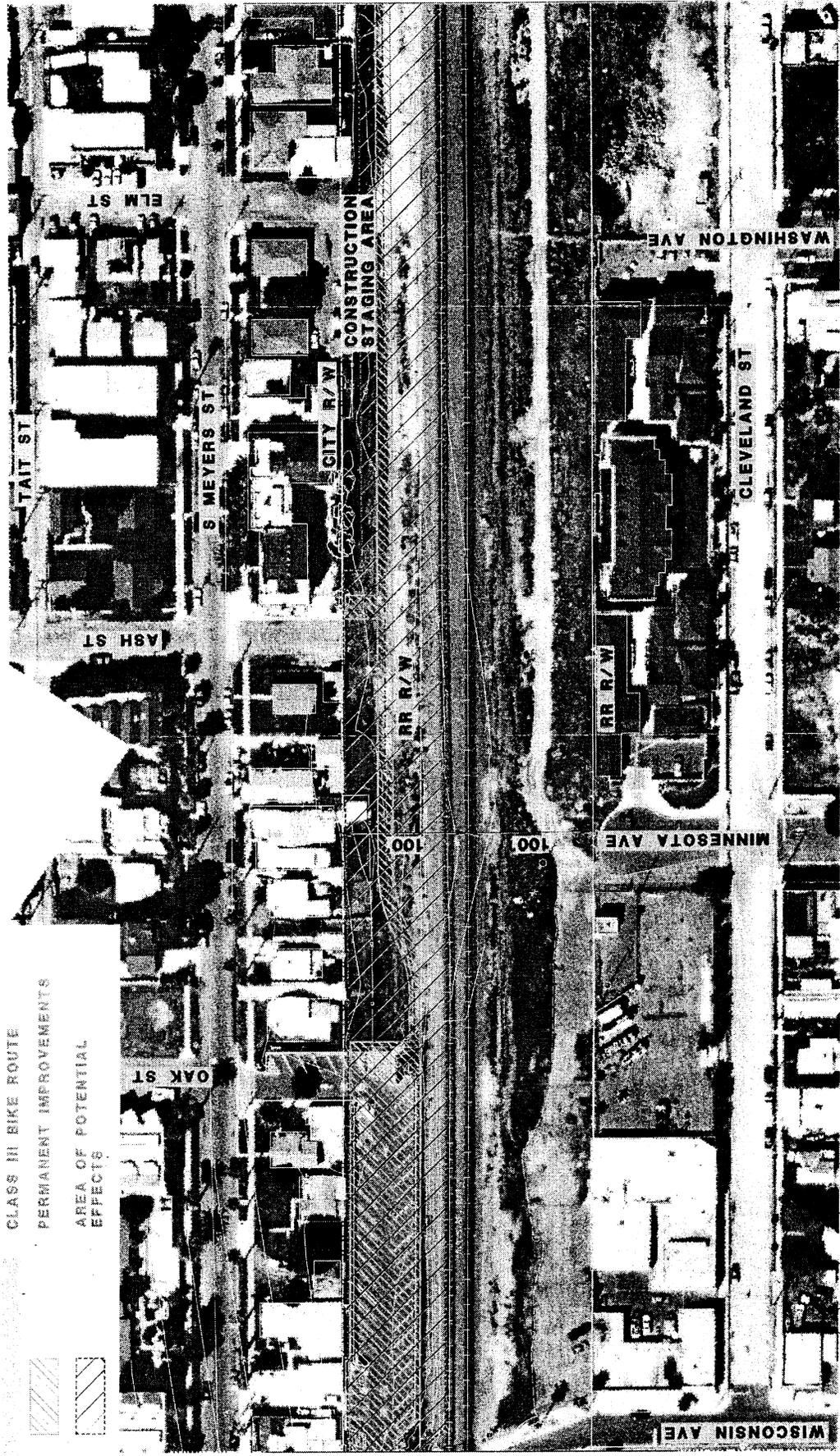
LEGEND

CLASS I BIKE PATH ALIGNMENT

CLASS III BIKE ROUTE

PERMANENT IMPROVEMENTS

AREA OF POTENTIAL EFFECTS



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PROJECT: COASTAL RAIL TRAIL
PHASE 2
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AREA OF
POTENTIAL EFFECTS
SHEET 5 OF 6
DEC 21, 2005

MATCH LINE-SEE SHEET 4

MATCH LINE-SEE SHEET 6

LEGEND

CLASS I BIKE PATH ALIGNMENT

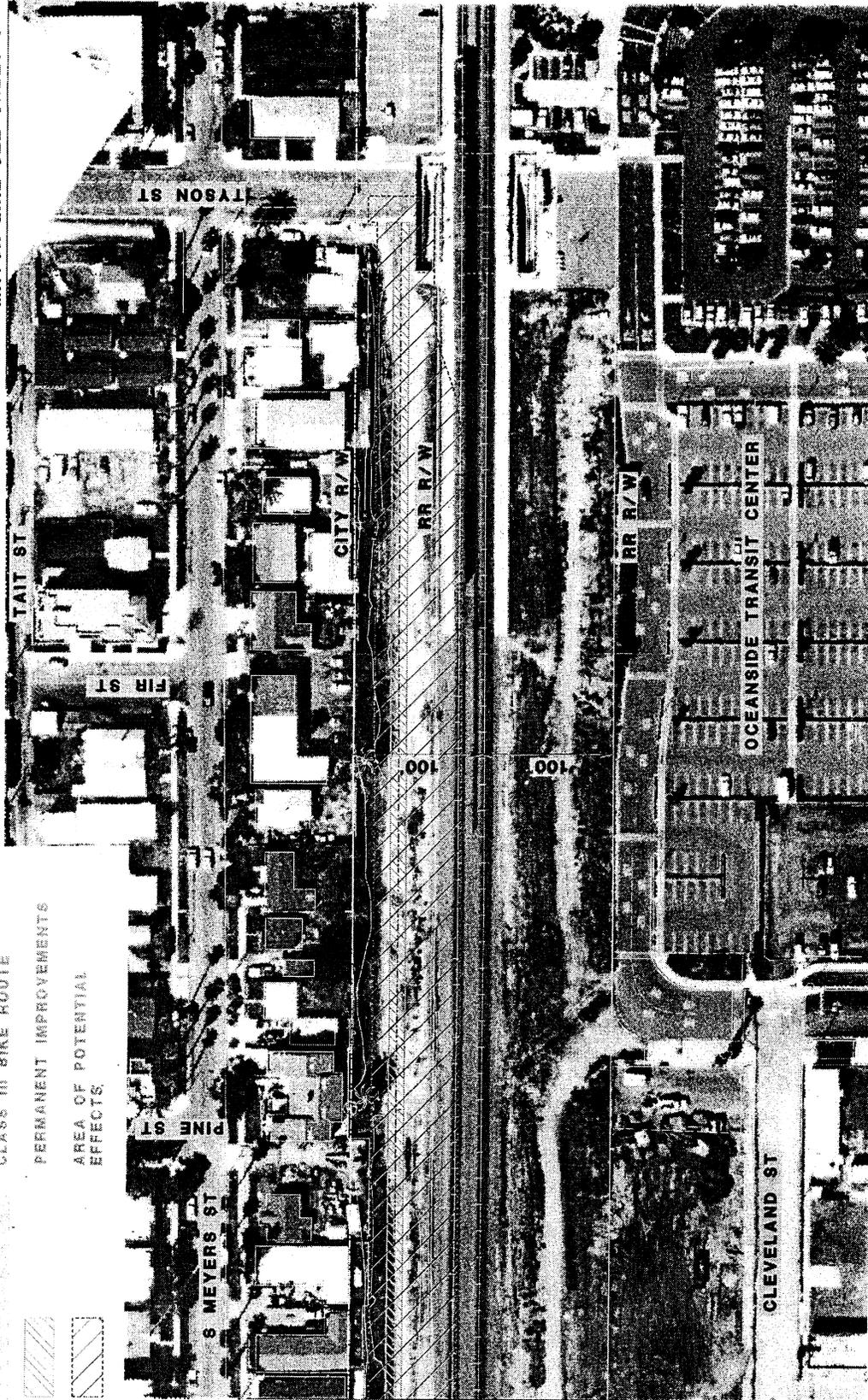
CLASS III BIKE ROUTE

PERMANENT IMPROVEMENTS

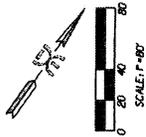
AREA OF POTENTIAL EFFECTS

MATCH LINE-SEE SHEET 7

NOTE:
NO PERMANENT IMPROVEMENTS OR ROADWAY STRIPING IS PROPOSED ALONG THE CLASS III BIKE ROUTE.
SIGNS TO DIRECT BIKE ROUTE WILL BE MOUNTED ON EXISTING LIGHT POLES OR SIGN POSTS.



MATCH LINE-SEE SHEET 5



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San Diego, CA 92123
(619) 514-8377

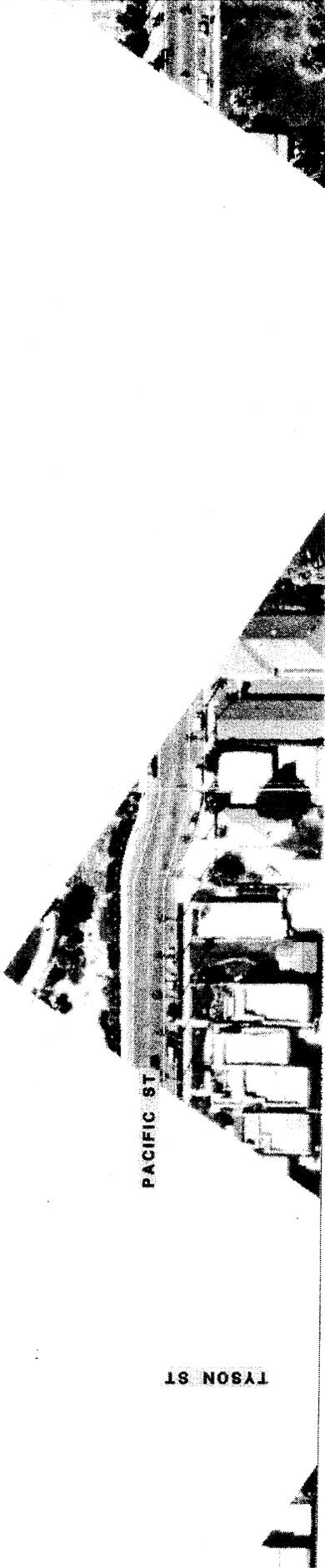
PROJECT: COASTAL RAIL TRAIL
PHASE 2
CITY OF OCEANSIDE
AREA OF
POTENTIAL EFFECTS
SHEET 6 OF 9
DEC 21, 2005

MATCH LINE-SEE SHEET 8

LEGEND

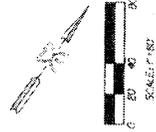
- CLASS I BIKE PATH ALIGNMENT
- CLASS III BIKE ROUTE

NOTE:
 NO PERMANENT IMPROVEMENTS OR ROWWAY STRIPING IS PROPOSED ALONG THE CLASS III BIKE ROUTE.
 SIGNS TO DIRECT BIKE ROUTE WILL BE MOUNTED ON EXISTING LIGHT POLES OR SIGN POSTS.



MATCH LINE-SEE SHEET 6

AERIAL FLOWN SPRING 2000



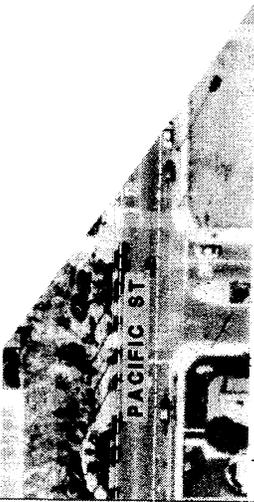
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PROJECT COASTAL RAIL TRAIL
 PHASE 2
 CITY OF OCEANSIDE

APE LIMITS
 SHEET 7 OF 9
 APRIL 28, 2005

MATCH LINE-SEE SHEET



PACIFIC ST



PIER VIEW WAY

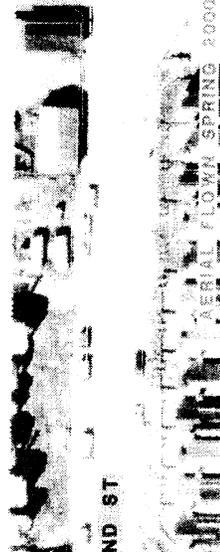
MATCH LINE LEFT

MATCH LINE RIGHT



PIER VIEW WAY

CLEVELAND ST



MATCH LINE SHEET 9

LEGEND

- CLASS I BIKE PATH ALIGNMENT
- CLASS III BIKE ROUTE

NOTE: NO PERMANENT IMPROVEMENTS OR ROADWAY STRIPING IS PROPOSED ALONG THE CLASS III BIKE ROUTE. SIGNS TO DIRECT BIKE ROUTE WILL BE MOUNTED ON EXISTING LIGHT Poles OR SIGN POSTS.



3665 Chesapeake Drive, Suite 415 San Diego, CA 92123 619-594-8377

PROJECT COASTAL RAIL TRAIL PHASE 2 CITY OF OCEANSIDE

APE LIMITS SHEET 8 OF 9 APRIL 26, 2005



SCALE: 1"=80'

END 'CRT' PHASE 2

SAN LUIS REY RIVER TRAIL

NEPTUNE WAY

AERIAL FLOWN SPRING 2000

WINDWARD WAY

CLEVELAND ST

SPORTSFISHER WAY

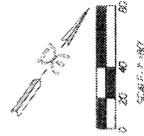
MATCH LINE-SEE SHEET 8

LEGEND

CLASS I BIKE PATH ALIGNMENT

CLASS III BIKE ROUTE

NOTES:
NO PERMANENT IMPROVEMENTS OR ROADWAY STRIPING IS PROPOSED ALONG THE CLASS III BIKE ROUTE.
SEMS TO DIRECT BIKE TRAFFIC WILL BE MOUNTED ON EXISTING LIGHT POLES OR SEMI POSTS.



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PROJECT: COASTAL RAIL TRAIL
PHASE 2
CITY OF OCEANSIDE

APE LIMITS
SHEET 8 OF 9
APRIL 28, 2005

Description of Currently Proposed Project

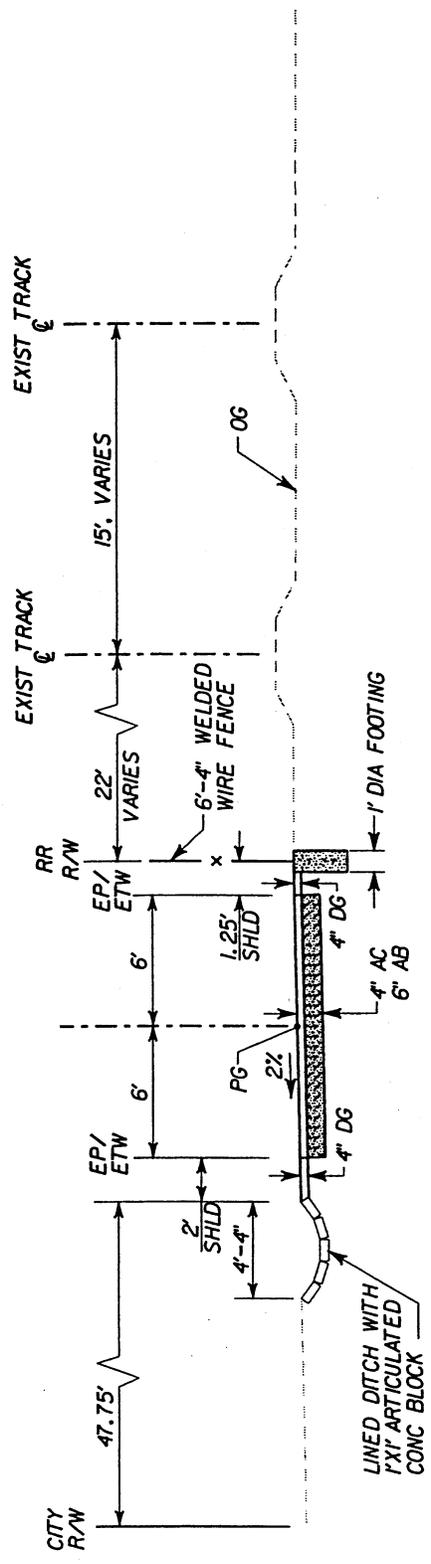
The northern segment of the Coastal Rail Trail will begin as a Class III bike route at Cleveland Street and Neptune Way. The Class III bike route will continue on Cleveland Street and Pier View Way. At Pier View Way, the Class III bike route will turn to the west, cross under the Pier View Way pedestrian undercrossing, and continue to Pacific Street. The route will then head south on Pacific Street. At Tyson Street, the route will turn to the east and travel along Tyson Street until reaching the connection with the Class I bike path. Signs for the Class III bike route will be mounted on existing light poles or sign posts. A typical sign showing the existing signage and rail trail information to be added is shown on the cover of the MND. No permanent improvements or roadway striping is proposed along the Class III bike route.

The Class I bike path will head south within the City right-of-way from Tyson Street to Wisconsin Avenue on the west side of the railroad tracks. Just north of Wisconsin, the trail will be located on the east side of an existing City pay parking lot. The parking lot will be restriped and one additional parking space will be created (more detail regarding this component is provided below). The trail will continue south and cross Wisconsin Avenue in a marked crosswalk adjacent to the parking lot entrance. South of Wisconsin Avenue, the trail will be located within the NCTD right-of-way, between adjacent residences and the existing railroad tracks. As part of the project, a 6-foot, 4-inch welded wire fence would be constructed between the bike path and the railroad to prevent unauthorized access (Figure 4). Existing private fences will be protected in place on the west side of the path. The bike path will continue south in the NCTD right-of-way to Oceanside Boulevard where there will also be a marked crosswalk for safe bike crossing.

A Class III bike route will then continue west on Oceanside Boulevard and south on Pacific Street, where it will connect to the southern Oceanside segment of the trail.

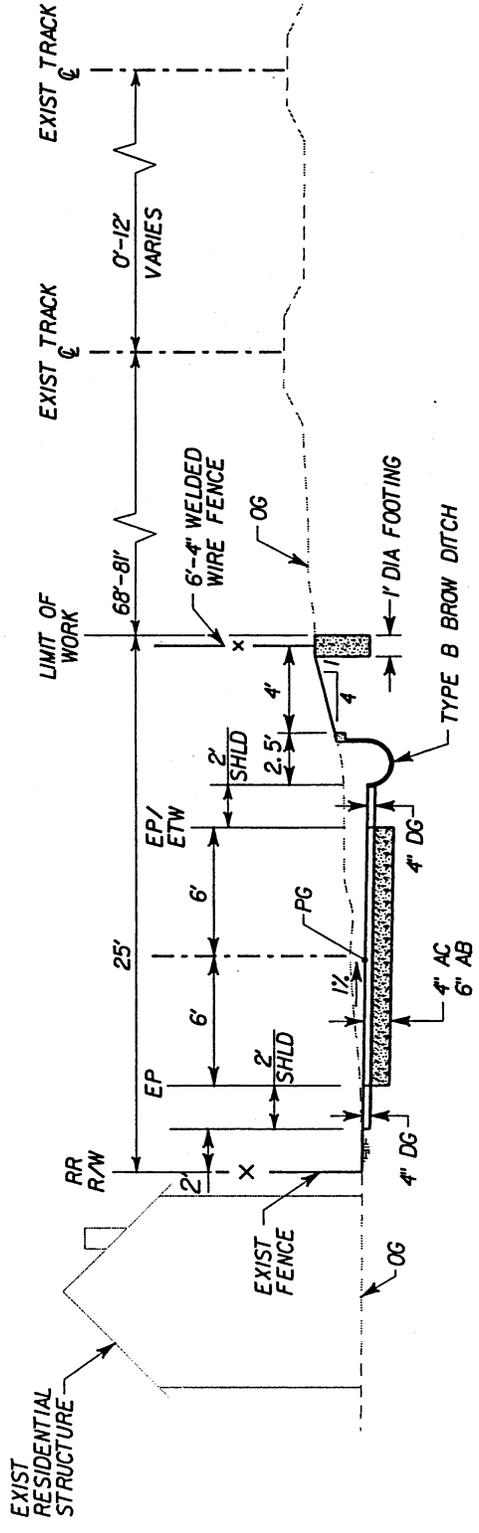
The Class I facility between Tyson Street and Oceanside Boulevard is shown on sheets 3 through 6 of the project maps. Detailed drawings including engineering cross-sections, grading and drainage improvements, signage, irrigation and landscaping, and lighting are available at the City Planning Department. As visible in these aerial photographs, there is an undeveloped swath between the railroad tracks and the existing homes to the west. This provides an ideal location to construct the bike path with minimal cross-traffic. The key access points will be at Tyson Street (northern ingress/egress at dead-end street), Wisconsin Street (middle ingress/egress with cross-traffic) and Oceanside Boulevard (southern ingress/egress with cross-traffic). There will be minor infrastructure improvements at Ash Street and Hayes Street and this will allow access to the bike path at these points.

The two key ingress/egress streets with cross-traffic, Oceanside Boulevard and Wisconsin Avenue, will have small medians at the termini of the path. The medians will separate the direction of travel, ensure that the trail users are traveling on the correct side of the trail, and bring cyclists to a stop before crossing the street. Directional and safety-related signs, such as stop signs, will be posted in all directions at the Oceanside Boulevard and Wisconsin Avenue street crossings. Street lights will also be installed at the medians and the medians will be landscaped. As shown in Figure 5, the median will support trees (typically 2 or 3 trees at each terminus) with a mixture of shrubs/groundcover below. The landscaping will be irrigated at this location. The medians will not have any decorative hardscape features or seating elements.



TYPICAL SECTION

COASTAL RAIL TRAIL
 GENERALLY WISCONSIN AVE TO TYSON ST
 NOT TO SCALE



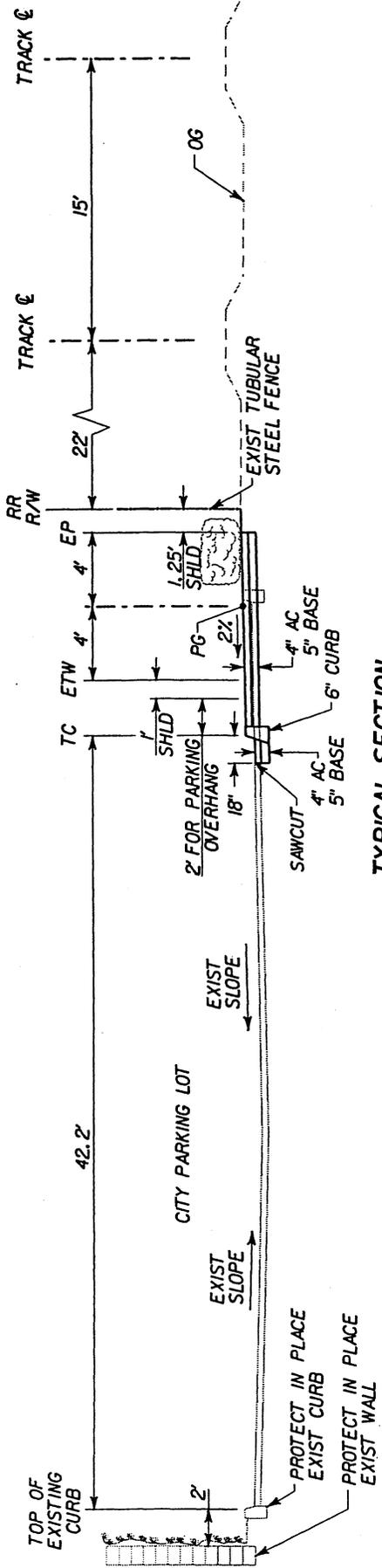
TYPICAL SECTION

COASTAL RAIL TRAIL
 GENERALLY OCEANSIDE BLVD TO WISCONSIN AVE
 NOT TO SCALE

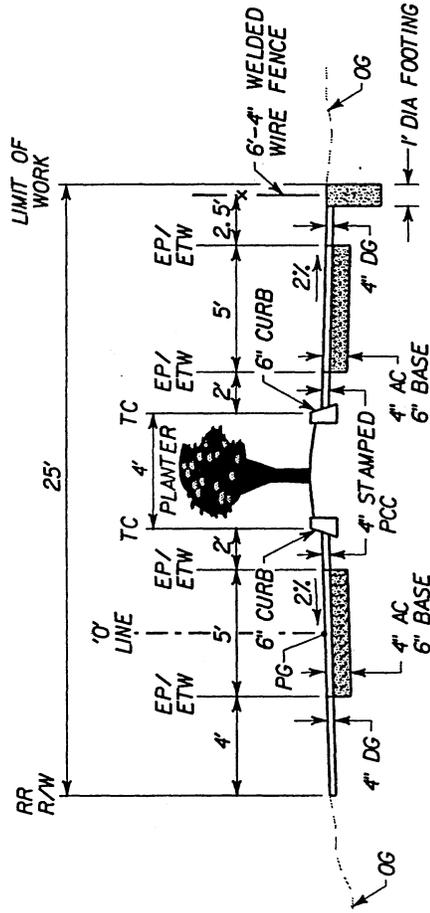
FIGURE 4

DEPOKKEN
 ENGINEERING
 5555 Chesapeake Drive, Suite 435
 San Diego, CA 92123
 (619) 594-8377

COASTAL RAIL TRAIL
 PHASE 2
 CITY OF OCEANSIDE
 PROJECT LIMITS
 TYPICAL SECTIONS
 FEB 11, 2005



TYPICAL SECTION
 COASTAL RAIL TRAIL
 CITY PARKING LOT
 NORTH OF WISCONSIN AVE
 NOT TO SCALE



TYPICAL SECTION
 COASTAL RAIL TRAIL
 LANDSCAPED MEDIAN
 TERMINI OF OCEANSIDE BLVD
 AND WISCONSIN AVE
 NOT TO SCALE

FIGURE 5

DEDOKKEN
 ENGINEERING
 9655 Chesapeake Drive, Suite 415 San Diego, CA 92123 (619) 544-6377

COASTAL RAIL TRAIL
 PHASE 2
 CITY OF OCEANSIDE
 PROJECT LIMITS
 TYPICAL SECTIONS
 FEB 11, 2006

Between Oceanside Boulevard and Wisconsin Avenue, where the bike path will be generally adjacent to private property fencelines, there are 4-foot-wide planting areas along either side of the bike path and an approximately 2-foot-wide concrete brow ditch to facilitate drainage (Figure 4). The bike path itself would be asphalt/concrete (AC) material for a smooth riding surface. Decomposed granite (DG) will buffer each side of the path. Adjacent planting areas will not be irrigated (Figure 4).

Between Wisconsin Avenue and Tyson Street, the path will be approximately 60 feet east of the property lines of adjacent homes. Here drainage will be facilitated by a 4-foot-wide ditch that is lined with articulated concrete blocks (Figure 4). Approximately 20 percent of the block area will remain open to facilitate establishment of vegetation for a more natural appearance. The only formal landscaping along this segment will be at the key cross-traffic intersections and the restriped parking lot. Irrigation will be provided at these locations. The path will also have an AC base with DG on the side.

The plant species to be used for landscaping are listed in Table 1. The bike path median trees will be Mexican fan palms. Other shrub and vine species to be used for landscaping along the bike path include native and drought-tolerant species.

**Table 1
Plant Species for CRT Landscaping**

| Common Name | Size |
|------------------------------|--------|
| Shrubs | |
| Coastal Sagebrush | 1 Gal. |
| Coastal Quail Bush | 1 Gal. |
| Dwarf Coyote Brush | 1 Gal. |
| Ceanothus | 1 Gal. |
| Giant Coreopsis | 1 Gal. |
| California Evening Primrose | 1 Gal. |
| Catalina Cherry | 1 Gal. |
| Rhus Integrifolia | 1 Gal. |
| Fuchsia-Flowering Gooseberry | 1 Gal. |
| Matilija Poppy | 1 Gal. |
| Vines | |
| Virgin's Bower | 1 Gal. |
| San Diego Honeysuckle | 1 Gal. |
| Southern California Grape | 1 Gal. |

At Ash Street, a paved path will connect the CRT path with the existing paved roadway. The articulated-concrete drainage ditch will drain the northern half of the Class I path to this location where it will connect to the existing curb and gutter system in Ash Street. There will also be a 16-foot-long gate in the fence on the east side of the path to allow NCTD access.

The brow ditch between Oceanside Street and Wisconsin Avenue will drain toward an inlet at Hayes Street. As part of the project, an 18 inch concrete pipe storm drain will be installed within Hayes Street to a connection at Pacific Street. This will divert the trail runoff into the existing storm drain system.

The trail width will vary depending upon location, but it will be primarily 12 foot wide, with six foot travelways in each direction. The northernmost end, from Tyson Street to Fir Street, will begin adjacent the Oceanside Transit Center pedestrian undercrossing. An existing tubular steel fence to the east will serve as a physical barrier between the trail and the tracks.

From Fir Street to the City pay parking lot on the north side of Wisconsin Avenue, a 6-foot, 4-inch-high, welded wire mesh fence will connect to the existing tubular steel fence to serve as a physical barrier between the path and tracks.

Adjacent to the city pay parking lot, the trail will narrow to 8 feet wide (4 foot travel lanes in each direction) with 1-foot-wide shoulders. The trail will be protected behind a constructed 6-inch curb and 2-foot parking overhang buffer and the physical barrier will be the existing steel tubular fence. The existing wall on the west side of the lot will be protected in place (see Figure 5).

From Wisconsin Avenue south to Oceanside Boulevard, the trail will be 12 feet wide with a 2-foot-wide shoulder on each side. The edge of the western shoulder will be 2 feet from the existing fence line separating the NCTD right-of-way from private property. At the edge of the eastern shoulder, there will be a 2 foot 6 inch wide brow ditch and total 8-foot 6-inch separation from the trail edge of pavement to the welded wire mesh fence (Figure 4).

The city owns and operates a small pay parking lot just north of Wisconsin Avenue that would be restriped to allow the bike path along its eastern limits. The parking lot allows only one way traffic with ingress from Wisconsin Avenue and egress on Oak Street. This pattern will remain, but the parking lot will be restriped so that parking spaces are shorter (small or compact vehicles) and the existing curb/landscape system will be redesigned. The lot will have an aisle-width of 15 feet which is narrower than the existing lot, but meets city standards. Landscaping will be provided in two small planters between parking spaces on the east side of the lot, and at two larger planters near the Oak Street egress. The smaller planters would have shrubs and the larger planters will have tipu trees (total of three trees). Street lighting will be provided in the parking lot.

Construction staging will occur within the disturbed City right-of-way between Ash Street and Elm Street.

As part of the project, six existing non-native trees will be removed between Oceanside Boulevard and Wisconsin Avenue. Two eucalyptus trees would be removed just south of Hayes Street and four junipers will be removed approximately 400 feet south of Wisconsin Avenue (see Figure 3, sheet 4 of 9). Project landscaping will result in installation of eight palm trees and three tipu trees for a net gain of five trees.

As shown on the project aerials (Spring 2000) there are several places where unauthorized construction occurred in the city and railroad right-of-way. Between Oceanside Boulevard and Wisconsin Avenue, the bike path will be located in the areas appearing to have fences and other structures. In the time period since this aerial was taken, the City and NCTD have worked with property owners to remove unauthorized structures and currently none remain in the area to be used for the bike path.

Technical Studies Completed for Project

Biological Resources (Attachment A)

On May 8, 2003, EDAW, Inc. wildlife biologist Melissa Wilson conducted a habitat assessment of the Class I bike path area from Tyson Street south to Oceanside Boulevard to determine the potential existence of threatened or endangered plant and animal species as well as the potential for raptor nesting.

The proposed alignment of the Class I bike path is highly disturbed. Although several invertebrates and small mammals were observed, no threatened or endangered species were observed or expected.

No evidence of current or past raptor nests were observed during the habitat assessment. Many other avian species were observed, though none of these are considered sensitive by resource agencies. The trees that occur within the planned pathway area provide potential perching and nesting sites for raptors. Also, many prey species exist near the project area that could support raptor species. Therefore, there is a high potential for raptors to nest in the trees that are located within the planned pathway.

Because construction of the proposed path will eliminate six of the trees within the path alignment, a preconstruction survey for potential nesting raptor species should be performed at least one week prior to construction. If all trees were removed at one time (e.g., within a few days of each other), additional raptor surveys will not be required. However, if any trees remain, raptor surveys should be performed once a month during the raptor-breeding season (February 1 to August 31) until construction is completed. If nesting raptors are observed in the planned pathway area, construction should be discontinued while the nest is active.

Cultural Resources (Attachment B)

A Negative Historic Properties Survey Report was prepared that concluded that none of the parcels within the APE of the proposed bike path have historic resources. An archaeological survey was conducted, and an Archaeological Survey Report was prepared. No cultural resources were identified within the project APE; therefore, no impacts will occur.

Hazardous Materials (Attachment C)

An Initial Site Assessment was prepared for the entire length of the Coastal Rail Trail in Oceanside, including the northern segment proposed for realignment. A site survey was conducted, and government records of any known hazardous waste sites, incidents, or generators within a 1.6-kilometer (1-mile) radius were obtained. It was determined that there are no known hazardous waste sites within or immediately adjacent to the proposed bike path between Tyson Street and Oceanside Boulevard.

9. Surrounding land uses and setting: (Briefly describe the project's surroundings.)

The project alignment will travel through highly urbanized areas of Oceanside. The Class III bike route along Pier View Way, Pacific Street, and Tyson Street will be aligned through a generally residential neighborhood on existing City streets. The paved streets will not be modified and will

remain in their existing condition. Between Tyson Street and Oceanside Boulevard the Class I bike path will travel adjacent to single-family and multifamily residential development, as well as some commercial uses. Most properties lining the railroad right-of-way have fencing separating the property from the railroad tracks.

10. Other public agencies whose approval is required: (e.g., permits, financing approval, or participation agreement.)

The proposed Class I bike path between Tyson Street and Oceanside Boulevard will be located within the NCTD and City rights-of-way. NCTD approval is required.

The project alignment is located within the California Coastal Zone, in Oceanside's Local Coastal Program area. The city will issue a coastal development permit via the Planning Commission and Community Development process. The portion of the storm drain installation in Hayes Street, west of Myers Street, will occur within the California Coastal Commission's "Appeal Jurisdiction."

Upon completion of this CEQA evaluation, appropriate environmental review will be completed under the National Environmental Policy Act (NEPA). Caltrans will act as the Local Assistance Liaison for the Federal Highway Administration who is the NEPA lead agency. The NEPA document is a Categorical Exclusion. A Coastal Consistency Determination (CCD) has also been prepared to satisfy federal regulations for proposed development in the coastal zone (Attachment D).

A Storm Water Mitigation Plan will also be required because the project will generate over 5,000 square feet of paved surface. This Plan must be approved by the City.

ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED:

The environmental factors checked below would be potentially affected by this project, involving at least one impact that is a "Potentially Significant Impact" as indicated by the checklist on the following pages.

- | | | |
|--|---|---|
| <input type="checkbox"/> Aesthetics | <input type="checkbox"/> Agricultural Resources | <input type="checkbox"/> Air Quality |
| <input checked="" type="checkbox"/> Biological Resources | <input type="checkbox"/> Cultural Resources | <input type="checkbox"/> Geology/Soils |
| <input type="checkbox"/> Hazards & Hazardous Materials | <input type="checkbox"/> Hydrology/Water Quality | <input type="checkbox"/> Land Use/Planning |
| <input type="checkbox"/> Mineral Resources | <input type="checkbox"/> Noise | <input type="checkbox"/> Population/Housing |
| <input type="checkbox"/> Public Services | <input type="checkbox"/> Recreation | <input type="checkbox"/> Transportation/Traffic |
| <input type="checkbox"/> Utilities/Service Systems | <input type="checkbox"/> Mandatory Findings of Significance | |

DETERMINATION: (To be completed by the Lead Agency)

On the basis of this initial evaluation:

- I find that the proposed project **COULD NOT** have a significant effect on the environment, and a **NEGATIVE DECLARATION** will be prepared.
- I find that although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because revisions to the project have been made by or agreed to by the applicant. A **MITIGATED NEGATIVE DECLARATION** will be prepared.
- I find that the proposed project **MAY** have a significant effect on the environment, and an **ENVIRONMENTAL IMPACT REPORT** is required.
- I find that the proposed project **MAY** have a "potentially significant impact" or "potentially significant unless mitigated" impact on the environment, but at least one effect (1) has been adequately analyzed in an earlier document pursuant to applicable legal standards, and (2) has been addressed by mitigation measures based on the earlier analysis as described on attached sheets. An **ENVIRONMENTAL IMPACT REPORT** is required, but it must analyze only the effects that remain to be addressed.
- I find that although the proposed project could have a significant effect on the environment, because all potentially significant effects (a) have been analyzed adequately in an earlier EIR or **NEGATIVE DECLARATION** pursuant to applicable standards, and (b) have been avoided or mitigated pursuant to that earlier EIR or **NEGATIVE DECLARATION**, including revisions or mitigation measures that are imposed upon the project, nothing further is required.

Signature 

6/13/05
Date of Final

Signature 

3/17/05
Date of Draft

Printed Name Jerry Hittleman

EVALUATION OF ENVIRONMENTAL IMPACTS:

1. A brief explanation is required for all answers except “No Impact” answers that are adequately supported by the information sources a lead agency cites in the parentheses following each question. A “No Impact” answer is adequately supported if the referenced information sources show that the impact simply does not apply to projects like the one involved (e.g., the project falls outside a fault rupture zone). A “No Impact” answer should be explained where it is based on project-specific factors as well as general standards (e.g., the project will not expose sensitive receptors to pollutants, based on a project-specific screening analysis).
2. All answers must take account of the whole action involved, including off-site as well as on-site, cumulative as well as project-level, indirect as well as direct, and construction as well as operational impacts.
3. Once the lead agency has determined that a particular physical impact may occur, then the checklist answers must indicate whether the impact is potentially significant, less than significant with mitigation, or less than significant. “Potentially Significant Impact” is appropriate if there is substantial evidence that an effect may be significant. If there are one or more “Potentially Significant Impact” entries when the determination is made, an EIR is required.
4. “Negative Declaration: Less Than Significant With Mitigation Incorporated” applies where the incorporation of mitigation measures has reduced an effect from “Potentially Significant Impact” to a “Less Than Significant Impact.” The lead agency must describe the mitigation measures, and briefly explain how they reduce the effect to a less than significant level.
5. Earlier analyses may be used where, pursuant to the tiering, program EIR, or other CEQA process, an effect has been adequately analyzed in an earlier EIR or negative declaration. Section 15063(c)(3)(D). In this case, a brief discussion should identify the following:
 - a) Earlier Analysis Used. Identify and state where they are available for review.
 - b) Impacts Adequately Addressed. Identify which effects from the above checklist were within the scope of and adequately analyzed in an earlier document pursuant to applicable legal standards, and state whether such effects were addressed by mitigation measures based on the earlier analysis.
 - c) Mitigation Measures. For effects that are “Less than Significant with Mitigation Measures Incorporated,” describe the mitigation measures, which were incorporated or refined from the earlier document and the extent to which they address site-specific conditions for the project.
6. Lead agencies are encouraged to incorporate into the checklist references to information sources for potential impacts (e.g., general plans, zoning ordinances). Reference to a previously prepared or outside document should, where appropriate, include a reference to the page or pages where the statement is substantiated.
7. Supporting Information Sources: A source list should be attached, and other sources used or individuals contacted should be cited in the discussion.
8. This is only a suggested form, and lead agencies are free to use different formats; however, lead agencies should normally address the questions from this checklist that are relevant to a project’s environmental effects in whatever format is selected.
9. The analysis of each issue should identify:
 - a) the significance criteria or threshold, if any, used to evaluate each question; and
 - b) the mitigation measure identified, if any, to reduce the impact to less than significance

I. AESTHETICS - Would the project:

- a. Have a substantial adverse effect on a scenic vista?

The project alignment is not located within a scenic vista, although views to the Pacific Ocean are protected by the city’s Local Coastal Plan. The project primarily consists of signage on roads and a new paved path for bicyclists which are very low in scale and hardly visible. There would be some new security fencing in the NCTD right-of-way, which would be landscaped as appropriate, but in these areas there is already existing fencing along adjacent residences. The new fence would be parallel to the existing fences. View corridors down streets would not be affected by this fence. Therefore, no impacts on scenic vistas will occur.

- b. Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway?

The project alignment is not located along or near a state scenic highway and, therefore, will not impact any visual resources along a designated state scenic highway (Caltrans 2003). Construction of the trail will require the removal of six trees along the alignment. The trees will be replaced at a greater than 1.5:1 ratio at key focal points along the path alignment. The removal of the existing trees will not result in a significant visual impact to the area.

The Class I bike path between Tyson Street and Oceanside Boulevard has a comprehensive landscape and streetscape design that will add visual character to a currently undeveloped area. The paved bike path will include fencing and landscaping in accordance with the landscaping design plan. (The landscape plan is available at the City Planning Department.) This segment of the trail will be vegetated with native and drought-tolerant shrubs and groundcover. Certain unique areas along the bike path, such as points where the trail enters or leaves the Class I alignment, will include features such as lighting, trees and container plants. The project will not result in a significant visual impact to the area. Construction activities associated with construction of the bike path will be short-term, and this temporary visual impact will not be significant.

The Class III bike route along Pier View Way, Pacific Street, and Oceanside Boulevard will not include any improvements or modifications other than the addition of identifying signs. No visual change or impact will result.

- c. Substantially degrade the existing visual character or quality of the site and its surroundings?

The construction of the bike path will not degrade the existing visual character or quality of the site or its surroundings. Please see the discussion above under I.b.

- d. Create a new source of substantial light or glare, which would adversely affect day or nighttime views in the area?

Focused lighting will be included as part of the Class I bike path. This lighting will be pedestrian level lighting included in the project as a safety feature only at key transition points (e.g., at the

Oceanside Boulevard and Wisconsin Avenue crossings and at the city-owned parking lot). The lighting will be dim and down-shielded to reduce upwards glare and light pollution. Therefore, no adverse effects on day or nighttime views will occur due to increased light and glare.

II. AGRICULTURAL RESOURCES - In determining whether impacts to agricultural resources are significant environmental effects, lead agencies may refer to the California Agricultural Land Evaluation and Site Assessment Model (1997) prepared by the California Department of Conservation as an optional model to use in assessing impacts on agricultural and farmland. Would the project:

- a. Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?

The project alignment is not in an area designated as Prime Farmland, Unique Farmland, or Farmland of Statewide Importance. The project will travel through urban areas. No farmland will be converted to urban uses.

- b. Conflict with existing zoning for agricultural use, or a Williamson Act contract?

The project alignment is not zoned for agriculture use nor is it part of a Williamson Act contract and therefore, will not conflict with zoning requirements.

- c. Involve other changes in the existing environment, which, due to their location or nature, could result in conversion of farmland to non-agricultural use?

The surrounding land is developed with urban uses and, therefore, the project will not change the existing environment resulting in a loss of farmland.

III. AIR QUALITY - Where available, the significance criteria established by the applicable air quality management or air pollution control district may be relied upon to make the following determinations. Would the project:

- a. Conflict with or obstruct implementation of the applicable air quality plan?

No significant source of stationary or mobile air pollutants will result from pedestrians or bicyclists using the trail. The facility may encourage beach users to walk or bicycle instead of drive, which could reduce automobile trips and be an air quality benefit. Temporary impacts will occur during construction of the project, but will be short term and will cease at the end of the construction phase. Impacts associated with project construction or operation will not conflict with implementation of applicable air quality plans.

- b. Violate any air quality standard or contribute substantially to an existing or projected air quality violation?

The project will not violate any air quality standards or contribute substantially to an existing or projected air quality violation because no significant source of pollutants is anticipated as described in III.a. above.

- c. Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard (including releasing emissions that exceed quantitative thresholds for ozone precursors)?

The project will cause short-term emissions of hydrocarbons, oxides of nitrogen, and particulate matter from temporary construction activities and employee vehicle trips. These impacts will not result in a cumulative considerable net increase of criteria pollutants.

- d. Expose sensitive receptors to substantial pollutant concentrations?

The project alignment will be located near trains and vehicle traffic but will not generate or expose people to substantial levels of air pollutants.

- e. Create objectionable odors affecting a substantial number of people?

The project will not create any significant levels of objectionable odors.

IV. BIOLOGICAL RESOURCES - Would the project:

- a. Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?

A Biological Habitat Assessment letter was prepared for the project and is included as Attachment A. No candidate, sensitive, or special status species are present along the project alignment. The report also concluded that the highly disturbed habitat along the project alignment will not support any sensitive species. No impact will result.

There are several non-native trees species (e.g., eucalyptus and juniper) that occur within and near the project alignment provide potential perching and nesting sites for raptors. Also, many prey species exist near the project area that could support raptor species. Therefore, there is a high

potential for raptors to nest in the trees that are located within the project alignment. The removal of six trees (four juniper and two eucalyptus) will be considered potentially significant, but will be mitigated to below a level of significant with implementation of mitigation measures specified in Section XVIII and summarized below. (See also Attachment A).

To avoid harming nesting raptors, a preconstruction survey shall be performed one week prior to construction. If nests are observed, construction shall be discontinued while the nest is active. If the survey is negative and the trees are removed within one week of each other, no additional survey are required. If the trees remain, raptor surveys should be performed once a month during the raptor-breeding season (February 1 to August 31) until construction is completed. If nesting raptors are observed in the planned pathway area, construction should be discontinued while the nest is active.

- b. Have a substantial adverse impact on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, or regulations or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?

As outlined in the Biological Habitat Assessment letter, the project alignment occurs along developed areas, and no riparian or other sensitive habitat has been identified in the immediate area. Therefore, the project will not have an adverse impact on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service.

- c. Have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, and other means?

The project alignment is not located within or adjacent to any wetland habitats, as defined by Section 404 of the Clean Water Act. Therefore, no impacts to wetlands will result.

- d. Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?

The project alignment will travel through developed urban areas that do not serve as, or support, a wildlife corridor. The alignment is not located near any wildlife nursery sites. Therefore, the project will not impact the movement of wildlife or impede use of native areas.

- e. Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?

The project will not conflict with any local policies or ordinances protecting biological resources because the project alignment passes through developed urban areas and generally has a very low biological resource value. The six trees to be removed will be replaced at a 1:1 ratio. There is no tree preservation ordinance in effect at this site.

- f. Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Communities Conservation Plan, or other approved local, regional, or state habitat conservation plan?

The project will not conflict with provisions of an adopted Multiple Habitat Conservation Program or other approved local, regional, or state habitat conservation plan because the project alignment is not within any adopted conservation plan.

V. CULTURAL RESOURCES - Would the project:

- a. Cause a substantial adverse change in the significance of a historical resource as defined in § 15064.5?

A Negative Historic Properties Survey Report was prepared that concluded that none of the parcels within the APE of the proposed bike path have historic resources.

- b. Cause a substantial adverse change in the significance of an archaeological resource pursuant to § 15064.5?

The project is located within a disturbed and highly urbanized area. An archaeological survey was conducted, and an Archaeological Survey Report was prepared. No cultural resources were identified within the project APE; therefore, no impacts will occur.

- c. Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?

The project is located within a disturbed and highly urbanized area. In addition, no unique resources were identified along the project alignment. Therefore, no direct or indirect impacts to unique resources will occur.

- d. Disturb any human remains, including those interred outside of formal cemeteries?

The project alignment is not located near any known prehistoric settlements, which is where human remains tend to be located. It is expected that the project will not disturb any human remains, including those interred outside of formal cemeteries.

VI. GEOLOGY AND SOILS - Would the project:

a. Expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving:

- | | | | | |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|
| i) Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42. | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|

According to the Alquist-Priolo Earthquake Fault Zoning Map, the project is not located near a known fault, and Oceanside is not listed as a City potentially affected by the earthquake fault zones (Dept. of Conservation 1997). The nearest known active fault is the northern extension of the Rose Canyon fault located approximately 12.9 kilometers (8 miles) to the west (offshore). Other faults in the area are two concealed faults located approximately 8 kilometers (5 miles) southeast and 12.9 kilometers (8 miles) north of the project site. Therefore, there will be no substantial adverse effects due to a fault rupture.

- | | | | | |
|------------------------------------|--------------------------|--------------------------|--------------------------|-------------------------------------|
| ii) Strong seismic ground shaking? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|------------------------------------|--------------------------|--------------------------|--------------------------|-------------------------------------|

The project alignment could be susceptible to seismic ground shaking. Given that no new structures are planned for the project, impacts to people or structures as a result of strong ground movement will not occur.

- | | | | | |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|
| iii) Seismic-related ground failure, including liquefaction? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|

The project alignment is not located near a fault, nor is it within an Earthquake Fault Zone (Department of Conservation 1997). Therefore, the potential for ground failure related to seismic activity will be minimal.

- | | | | | |
|-----------------|--------------------------|--------------------------|--------------------------|-------------------------------------|
| iv) Landslides? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|-----------------|--------------------------|--------------------------|--------------------------|-------------------------------------|

The project alignment is located in an area that is considered only marginally susceptible to landslides (Department of Conservation 1995). Therefore, the potential for adverse impacts to occur to people or buildings due to landslides will not be significant

- | | | | | |
|---|--------------------------|--------------------------|-------------------------------------|--------------------------|
| b. Result in substantial soil erosion or the loss of topsoil? | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
|---|--------------------------|--------------------------|-------------------------------------|--------------------------|

The project alignment is relatively flat. During construction activities, standard best management practices will be used to minimize soil erosion. The Class I bike path will be paved. Graded or cut areas will be landscaped, thus eliminating the potential for substantial erosion or loss of topsoil. There will be no impacts from soil erosion.

- c. Be located on a geologic unit or soil that is unstable, or that will become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse?

The project alignment is located on terrace deposits consisting of Marina loamy coarse sand (USDA 1973). This soil type is well drained, and the area is only marginally susceptible to landslides. The bike path will be designed and built to meet all standard geologic safety requirements. Therefore, no impacts due to unstable soil or geology will result.

- d. Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial risks to life or property?

The type of soil underlying the project alignment is classified as Marina loamy coarse sand (USDA 1973). This soil type is not an expansive soil and the potential for shrink-swell behavior is limited. Therefore, the project will create no risk to human life or property due to expansive soils.

- e. Have soils incapable of adequately supporting the use of septic tanks or alternative waste water disposal systems where sewers are not available for the disposal of waste water?

The project does not involve the installation of septic tanks or alternative waster water disposal systems and therefore, will not have any impacts due to the use of septic systems or alternative wastewater disposal.

VII. HAZARDS AND HAZARDOUS MATERIALS - Would the project:

- a. Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?

The project proposes to construct a pedestrian/bicycle path that will not require the use, transport, or disposal of hazardous materials. Therefore, the project will not create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials. Additional information is provided in the Initial Site Assessment included in Attachment C.

- b. Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?

An Initial Site Assessment has been completed for this project (Attachment C). No hazardous materials, hazardous spills, or landfills have been identified on or directly adjacent to the project alignment. In addition, the project components will not involve the use or transportation of

hazardous materials. Therefore, the potential for accidental conditions involving the release of hazardous material into the environment are not anticipated to occur.

- c. Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?

There are two elementary schools located within 0.40 kilometer (0.25 mile) to the east of the project. However, the project will not include any land use or activity that will emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste, and of the project. Therefore, the project will have no potential effect on any nearby school related to hazardous material exposure.

- d. Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?

No hazardous sites have been recorded or identified within the project area; therefore, no significant impact will occur to the public or the environment.

- e. For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard for people residing or working in the project area?

The project is not located within two miles of an airport and, therefore, will not result in a safety hazard for people residing or working in the project area.

- f. For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people residing or working in the project area?

The project is not located within two miles of a private airstrip and, therefore, will not result in a safety hazard for people residing or working in the project area.

- g. Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?

The project will not alter, interfere with, or block any area roadways or emergency evacuation routes. Therefore, the project will not impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plans.

- | | | | | |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|
| h. Expose people or structures to a significant risk of loss, injury or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|

The project is not located near any areas of wildlands and will not expose people or structures to a significant risk of loss, injury, or death involving wildland fires.

VIII. HYDROLOGY AND WATER QUALITY - Would the project:

- | | | | | |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|
| a. Violate any water quality standards or waste discharge requirements? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|

Based on the type and limited duration of construction activities, the project design features intended to allow water infiltration and sediment retention, and the use of best management practices during the construction phase of the project, impacts to surface water quality from construction runoff will be minimal. The proposed bike path will result in an increased area of impervious surface approximately 0.23 hectare (0.57 acre) in size, generating a minimal increase in surface water runoff. The project will adhere to any waste discharge requirements and water quality standards. As required by the City, a Standard Urban Stormwater Mitigation Plan will be prepared to document the effectiveness of project drainage/run-off control features.

- | | | | | |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|
| b. Substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (i.e., the production rate of pre-existing nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted)? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|

The project will not require any use of groundwater because it will not be irrigated. The project will result in a very small increase in impervious surface, which will not substantially alter the recharge of groundwater supplies.

- | | | | | |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|
| c. Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, in a manner which would result in substantial erosion or siltation on- or off-site? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|

The project will maintain and follow the relatively flat topography of the project area. No streams or rivers will be altered by the project.

- | | | | | |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|
| d. Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, or substantially increase the rate or amount of surface runoff in a manner that would result in flooding on- or off-site? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|

The project will not substantially alter the existing drainage pattern of the site or area, including streams or rivers as discussed above. The project will not substantially increase the amount of surface runoff in a manner that will result in flooding. Runoff from the bike path will be controlled through project design.

- e. Create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff?

The project will not create or contribute runoff water that will exceed the capacity of planned or existing stormwater drainage systems, nor will it provide a substantial new source of polluted runoff.

- f. Otherwise substantially degrade water quality?

Drainage from the project will be managed in accordance with City standards and will not substantially degrade water quality.

- g. Place housing within a 100-year flood hazard area as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map?

The project will not create any residential housing; therefore, no impact will result.

- h. Place within a 100-year flood hazard area structures which would impede or redirect flood flows?

The project will not include any structures that could impede or redirect flood flows.

- i. Expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam?

The project alignment is not located in an area susceptible to flooding, nor is it near a levee or dam that could potentially result in inundation. The project will not expose people or structures to significant risk from flooding.

- j. Inundation by seiche, tsunami, or mudflow?

The project is not in an area susceptible to a seiche or mudflow. The project is within an area susceptible to a tsunami because it is close to the ocean; however, the project will not construct structures and will not result in an increased risk to humans or the surrounding environment if a tsunami were to occur.

IX. LAND USE AND PLANNING - Would the project:

- a. Physically divide an established community?

The project is a small segment of the Coastal Rail Trail, a 71-kilometer-long (44-mile-long) regional bicycle and pedestrian pathway/trail. The project will construct the trail in existing railroad and City right-of-way and will connect other recreational facilities along the route. This project is identified in the City's Recreational Trails and Land Use Elements of the General Plan and will not divide an established community.

- b. Conflict with any applicable land use plan, policy, or regulation of an agency with jurisdiction over the project (including, but not limited to the general plan, specific plan, local coastal program, or zoning ordinance) adopted for the purpose of avoiding or mitigating an environmental effect?

The project is included in the City's Land Use and Recreational Trails Elements of the General Plan. The project will follow the policies provided in the Oceanside Local Coastal Program and California Coastal Act. The project is consistent with the General Plan land use designations and the zoning ordinance.

- c. Conflict with any applicable habitat conservation plan or natural communities conservation plan?

The project alignment passes through a highly urbanized area and will not conflict with any applicable conservation plan.

X. MINERAL RESOURCES - Would the project:

- a. Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?

There are no known mineral resources underlying the project alignment. The area is highly urbanized and will not be appropriate for mineral extraction. Project construction will not result in the loss of availability of a known mineral resource.

- b. Result in the loss of availability of a locally-important mineral resource recovery site delineated on a local general plan, specific plan, or other land use plan?

The project alignment is not delineated on the City's General Plan, Land Use Element as a locally important mineral resource recovery site.

XI. NOISE - Would the project result in:

- a. Exposure of persons to or generation of noise levels in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?

The project will provide a pedestrian/bicycle pathway. Persons using these facilities will generate minimal noise that will not exceed established standards. Path users will be exposed to temporary noise from passing trains on the adjacent railroad tracks. Temporary construction noise will be managed in accordance with the City's noise ordinance.

- b. Exposure of persons to or generation of excessive groundborne vibration or groundborne noise levels?

The project will not expose persons to excessive groundborne vibration or groundborne noise levels because the only noise source will be bicyclists and pedestrians on non-motorized vehicles.

- c. A substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project?

Only minimal noise will be generated by the use of the trail by pedestrians and bicycles. No substantial permanent increase in ambient noise levels in the project vicinity will result.

- d. A substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project?

Temporary increases in ambient noise levels will occur during construction of the bike path. Because the increase in noise levels due to construction activities will be temporary and in accordance with City noise standards, the impact will be less than significant.

- e. For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?

The project is not located within two miles of an airport and, therefore, will not expose people residing or working in the project area to excessive aircraft noise levels.

- f. For a project within the vicinity of a private airstrip, would the project expose people residing or working in the project area to excessive noise levels?

The project is not located within two miles of a private airstrip and, therefore, will not expose people residing or working in the project area to excessive aircraft noise levels.

XII. POPULATION AND HOUSING - Would the project:

- a. Induce substantial population growth in an area, either directly (for example, by proposing new homes and business) or indirectly (for example, through extension of roads or other infrastructure)?

The project will provide a recreational trail through the City and does not propose any new housing or expand infrastructure that will induce substantial population growth.

- b. Displace substantial numbers of existing housing, necessitating the construction of replacement housing elsewhere?

There is no existing housing on the project alignment and, therefore, the project will not displace any housing necessitating the construction of replacement housing elsewhere.

- c. Displace substantial numbers of people, necessitating the construction of replacement housing elsewhere?

There is no existing residential use on the project alignment and, therefore, the project will not displace any people nor necessitate the construction of replacement housing elsewhere.

XIII. PUBLIC SERVICES

- a. Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:

Fire protection?

The project will construct no new structures that will require fire protection.

Police protection?

The project is a pedestrian/bicycle path. The recreational trail will not result in impacts to police protection service.

Schools?

The project will create a recreational opportunity through the City and will not generate students.

Parks?

The project will not require new parks because the project itself will result in a new recreational facility. The Class III bike route will pass near three community parks on Pacific Street; Rotary Park, Pacific Street Linear Park, and Tyson Street Park. The trail will provide access to these parks, but will not result in an overcrowding or overuse of these facilities.

Other public facilities?

No effects on other public facilities will occur from the project.

XIV. RECREATION

a. Would the project increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?

The Coastal Rail Trail will provide a recreational opportunity in the form of a pedestrian/bicycle trail. As described above in VIII.a., the Class III bike route will pass near three community parks along Pacific Street and will be near the beach. This trail will provide users with increased access to local and regional parks including the beach, but will not result in substantial deterioration of local recreation facilities.

b. Does the project include recreational facilities or require the construction or expansion of recreational facilities which might have an adverse physical effect on the environment?

The project is a recreational facility. Construction and operation of this project will not adversely affect the environment.

XV. TRANSPORTATION/TRAFFIC - Would the project:

a. Cause an increase in traffic which is substantial in relation to the existing traffic load and capacity of the street system (i.e., result in a substantial increase in either the number of vehicle trips, the volume to capacity ratio on roads, or congestion at intersections)?

The project will promote pedestrian and bicycle use. The project will not substantially increase the number of vehicle trips, volume-to-capacity ratio on roads, or congestion at intersections. It may encourage people to make trips via bike or walking instead of using their vehicle which will be a project benefit.

- b. Exceed, either individually or cumulatively, a level of service standard established by the county congestion management agency for designated roads or highways?

The project will generate minimal vehicular traffic only during construction. Therefore, the project will not exceed level of service standards for roadways in the project area. As discussed above in XV.a., the project may encourage alternative modes of transportation resulting in fewer vehicle trips.

- c. Result in a change in air traffic patterns, including either an increase in traffic levels or a change in location that results in substantial safety risks?

The project is not located near an airport and will not include any tall vertical features; therefore, no change in air traffic patterns causing substantial safety risks will result.

- d. Substantially increase hazards to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?

Pedestrians and bicyclists will be required to follow all applicable traffic rules and regulations when traveling on shared roadways. Project design incorporates safety measures at the two key intersections where cross-traffic would be perpendicular to the bike path.

- e. Result in inadequate emergency access?

The project will not block or alter access to any existing location and; therefore, will not result in inadequate emergency access.

- f. Result in inadequate parking capacity?

Just north of Wisconsin Avenue, the bike path will be located within an existing City of Oceanside pay parking lot. The bike path cross-section will be narrowed slightly (from 12 feet to 8 feet), and the entire parking lot will be restriped. This will result in a net gain of one parking space, although the length of individual spaces will be reduced. The parking lot is large enough to accommodate this restriping while maintaining City standards. Therefore, the project will not result in inadequate parking capacity.

- g. Conflict with adopted policies, plans, or programs supporting alternative transportation (e.g., bus turnouts, bicycle racks)?

The project will promote alternative transportation. The trail will provide a safe and continuous linear route for pedestrians and bicyclists.

XVI. UTILITIES AND SERVICE SYSTEMS - Would the project:

- a. Exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board?

The project will not generate wastewater; therefore, no impacts will result.

- b. Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?

The project will not generate wastewater. Water usage for irrigation of landscaping will not generate the need for new or expanded water facilities.

- c. Require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?

The bike path will incorporate any necessary drainage including brow ditches adjacent to the path. A drainage ditch will drain the trail area to Ash Street. Storm drain improvements are also proposed at Hayes Street.

- d. Have sufficient water supplies available to serve the project from existing entitlements and resources, or are new or expanded entitlements needed?

Sufficient water supplies are available to serve the project. New or expanded entitlements will not be needed.

- e. Result in a determination by the wastewater treatment provider which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?

The project will not generate wastewater and, therefore, no impact to wastewater treatment capacity will result.

- f. Be served by a landfill with sufficient permitted capacity to accommodate the project's solid waste disposal needs?

Operation of the project will not generate solid waste. Construction of the sidewalk and bike path could generate a small amount of solid waste that could be accommodated in local landfills.

- g. Comply with federal, state, and local statutes and regulations related to solid waste?

Operation of the project will not generate solid waste, and any construction waste will be disposed of in compliance with all applicable regulations.

XVII. MANDATORY FINDINGS OF SIGNIFICANCE

- a. Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal, or eliminate important examples of the major periods of California history or prehistory?

As discussed in Section IV, Biological Resources, the project alignment will pass through a highly developed urban area, but there is the potential to impact raptors that may be nesting in the area. This potential impact will be reduced to less than significant through the mitigation measures listed in Section XVIII below.

The project will not substantially impact habitat, populations, or range of plant or animal species. The project will not eliminate important examples of California history or prehistory because sensitive cultural resources are not present in the area of impact as outlined in Section V, Cultural Resources.

- b. Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects.)

Potentially significant impacts to biological resources could result. These impacts will be mitigated to below a level of significance by the mitigation measure outlined below. All other environmental issue areas were found to result in no impact or a less than significant impact. None of the potential impacts identified will result in cumulatively significant impacts.

- c. Does the project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly?

The project will not cause substantial adverse effects on human beings, either directly or indirectly. This conclusion is based on the above analysis that found potentially significant impacts in the issue area of Biological Resources. These impacts will be mitigated to below a level of significance and will not cause substantial effects on human beings, either directly or indirectly.

XVIII. MITIGATION MEASURES

Biological Resources

The removal of six trees (two eucalyptus and four juniper) could result in potentially significant impacts to nesting raptors. The following mitigation measures will reduce the potential impact to less than significant.

A preconstruction survey for potential nesting raptor species shall be performed at least one week prior to construction. If all trees were removed at one time (e.g., within a few days of each other), additional raptor surveys will not be required. However, if any trees remain, raptor surveys shall be performed once a month during the raptor-breeding season (February 1 to August 31) until construction is completed. If nesting raptors are observed in the planned pathway area, construction shall be discontinued while the nest is active.

The trees removed for path construction shall be replaced at a 1:1 ratio somewhere within the path alignment. As noted in the project description, landscaping plans indicate a greater than 1.5:1 replacement ratio so this measure has been fulfilled.

Level of Significance After Mitigation

The proposed mitigation will reduce the potential impacts to Biological Resources as described above to less than significant.

| Issues & Supporting Information Sources | Potentially Significant Impact | Potentially Significant With Mitigation Incorporated | Less Than Significant Impact | No Impact |
|---|--------------------------------|--|------------------------------|-----------|
|---|--------------------------------|--|------------------------------|-----------|

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XIX. REFERENCES

California Department of Conservation; Division of Mines and Geology

1997 Fault-Rupture Hazard Zones in California. Alquist-Priolo Earthquake Fault Zoning Act with Index to Earthquake Fault Zones Maps.

1995 Landslide Hazards in the Northern Part of the San Diego Metropolitan Area, San Diego County, California.

California Department of Transportation (Caltrans)

2003 website: <www.dot.ca.gov/hq/LandArch/scenic_highways/sdiego.htm>. Downloaded February.

California Coastal Commission

1998 *California Coastal Act of 1976*. Amended January.

City of Carlsbad

2001 Mitigated Negative Declaration. Coastal Rail Trail Project Oceanside to Del Mar. April.

City of Oceanside

1985 Local Coastal Program (Certified by Coastal Commission July 10)

1990 Noise Ordinance

1986 General Plan Land Use Element, September

1995a Recreational Trails Element, Oceanside General Plan

2000 Zoning Ordinance. November.

U.S. Department of Agriculture (USDA)

1973 Soil Survey: San Diego Area, California, Imperial Beach Quadrangle.

Wallace Roberts & Todd (WRT)

2002 Coastal Rail Trail, Oceanside. Landscape Amenity Concept 30% Schematic Design. May 24.

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ATTACHMENT A
BIOLOGICAL HABITAT ASSESSMENT LETTER

October 29, 2004

Jerry Hittleman
City of Oceanside
Planning Department
300 North Coast Highway
Oceanside, CA 92054

RE: Oceanside Coastal Rail Trail – Phase 2 Habitat Assessment

Project Description

The City of Oceanside (City) proposes to construct a segment of the Coastal Rail Trail, a 44-mile-long regional bicycle and pedestrian pathway/trail that will extend from Oceanside to downtown San Diego. The Oceanside segment of the Coastal Rail Trail is the northernmost portion and will begin in northern Oceanside at the western end of the San Luis Rey Recreational Trail. The trail will continue south through the City and connect to the City of Carlsbad segment of the Coastal Rail Trail. Approximately 3 miles of the Coastal Rail Trail will be located in Oceanside. The trail will be located within the North County Transit District (NCTD) railroad right-of-way, CITY right-of-way, or on CITY streets. Where the trail will be in or adjacent to railroad right-of-way, the trail will be constructed as a Class I bike path. Class I bike paths are facilities with exclusive rights-of-way, with minimum cross flows by motorists. Where the trail is located on CITY streets, signs will be posted to designate the trail route (Class III).

The proposed project is a Class I bike path adjacent to the west side of the NCTD railroad tracks between Tyson Street and Oceanside Boulevard, within CITY and NCTD rights-of-way. In order to route bicycle traffic to the west side of the railroad tracks, the northern segment of the Coastal Rail Trail will begin as a Class III bike route at Pier View Way and Cleveland Street. At Pier View Way, the Class III bike route will turn to the west and continue to Pacific Street. The route will then head south on Pacific Street. At Tyson Street, the route will turn to the east and travel along Tyson Street until reaching the connection with the Class I bike path. At that point, the trail will head south to Oceanside Boulevard within the City and NCTD rights-of-way on the west side of the railroad tracks as a Class I bike path. The trail will then continue west on Oceanside Boulevard and south on Pacific Street as a Class III route, where it will connect to the southern Oceanside segment of the trail.

The Class I bike path will head south within the CITY right-of-way from Tyson Street to Wisconsin Avenue on the west side of the railroad tracks. Just north of Wisconsin, the trail will be located on the east side of an existing Oceanside pay parking lot. The parking lot will be restriped so that no spaces will be lost. The trail will continue south and cross Wisconsin Avenue in a marked crosswalk adjacent to the parking lot entrance. South of Wisconsin Avenue, the trail will be located within a 25-foot-wide swath along the western edge of the NCTD right-of-way adjacent residences and continue south in the NCTD right-of-way to Oceanside Boulevard where there will be a marked crosswalk.

At Ash Street, a paved path will connect the Coastal Rail Trail path with the existing paved roadway dead end to the north. A drainage ditch will drain the trail area to Ash

Street and connect to the existing curb and gutter. At Hayes Street, storm drain improvements are proposed to drain the trail area away from existing residences and into the existing storm drain system.

Habitat Assessment

On May 8, 2003 EDAW, Inc. wildlife biologist Melissa Wilson conducted a habitat assessment for the proposed bike path between Tyson Street and Wisconsin Avenue to determine the potential for federally or state-listed threatened or endangered plant and animal species and the potential for raptor nesting to occur within and adjacent to the proposed bike path alignment. The habitat assessment was conducted on foot by walking the length of the proposed project area. Trees that may be impacted were scanned visually for the existence of past or present raptor nesting activity.

The habitat within the proposed bike path alignment is highly disturbed. Although many invertebrates and small mammals were observed during this survey, no federally or state-listed threatened or endangered species were observed or are expected to occur within or adjacent to the proposed bike path alignment.

No evidence of current or past raptor nesting was observed during the habitat assessment. Many avian species were observed during the survey, though none of these are listed by the resource agencies.

The trees that occur within and adjacent to the proposed bike path alignment provide potential perching and nesting sites for raptors. Also, many prey species exist within and adjacent to the project area that could support raptor species. Therefore, there is a high potential for raptors to nest in the trees that are located within the proposed sidewalk and bike path alignment.

Because construction of the proposed bike path will eliminate six of the trees within the trail alignment, a preconstruction survey for potential nesting raptor species should be performed within one week prior to construction. If all trees were removed at one time (e.g. within a few days of each other), additional raptor surveys would not be required. However, if any trees remain, raptor surveys should be performed once a month during the raptor-breeding season (February 1 to August 31) until construction is completed. If nesting raptors are observed within or adjacent to the proposed alignment, any construction within 300 feet of the nest should be discontinued while the nest is active.

If you have any questions or comments regarding this letter report, please contact me at (619) 233-1454.

Sincerely,



Lyndon Quon
Senior Wildlife Biologist

ATTACHMENT B
NEGATIVE ARCHAEOLOGICAL SURVEY REPORT
NEGATIVE HISTORIC PROPERTY SURVEY REPORT
(Bound Separately)

ATTACHMENT C
INITIAL SITE ASSESSMENT

**INITIAL SITE ASSESSMENT
FOR THE OCEANSIDE
COASTAL RAIL TRAIL PROJECT**

Prepared for:

City of Oceanside
300 North Coast Highway
Oceanside, California 92054

Prepared by:

EDAW, Inc.
1420 Kettner Boulevard, Suite 620
San Diego, California 92101

for:

Dokken Engineering
9665 Chesapeake Drive, Suite 435
San Diego, California 92123-1367

October 29, 2004

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Initial Site Assessment (ISA) Checklist

Project Information

District 11 County San Diego Route see description below Kilometer Post (Post Mile) N/A EA N/A

Description: The Oceanside segment of the Coastal Rail Trail is the northernmost portion and will begin in northern Oceanside at the western end of the San Luis Rey Recreational Trail. The trail will continue south through the City and connect to the Carlsbad segment of the Coastal Rail Trail. Approximately 5 kilometers (3 miles) of the Coastal Rail Trail will be located in the City. The trail will be located within the North County Transit District (NCTD) railroad right-of-way, City right-of-way, or on City streets.

The proposed project is the northern segment of the Oceanside Coastal Rail Trail. The trail will begin in northern Oceanside at Pier View Way and Cleveland Street and travel west on Pier View Way, south on Pacific Street, and east on Tyson Street as a Class III bike route. A Class I bike path will be constructed adjacent to the west side of the NCTD railroad tracks between Tyson Street and Oceanside Boulevard, within City and NCTD rights-of-way.

The Class I bike path will head south within the City right-of-way from Tyson Street to Wisconsin Avenue on the west side of the railroad tracks. Just north of Wisconsin, the trail will be located on the east side of an existing Oceanside pay parking lot. The parking lot will be restriped so that no spaces will be lost. The trail will then turn south and cross Wisconsin Avenue in a marked crosswalk adjacent to the parking lot entrance. South of Wisconsin Avenue, the trail will be located within a 25-foot-wide swath along the western edge of the NCTD right-of-way adjacent to residences and continue south to Oceanside Boulevard where there will also be a marked crosswalk.

Oceanside Boulevard and Wisconsin Avenue will have striped crosswalks with warning signs in both directions. At Ash Street, a paved path will connect the Coastal Rail Trail path with the existing paved roadway dead end to the north. A drainage ditch will drain the trail area to Ash Street and connect to the existing curb and gutter. At Hayes Street, storm drain improvements are proposed to drain the trail area away from existing residences and into the existing storm drain system.

Is the project on the HW Study Minimal-Risk Projects List (HW1)? No

Project Manager Jerry Hittleman phone # (760) 435-3535

Project Engineer Kirk Bradbury phone # (858) 514-8377

Project Screening

Attach the project location map to this checklist to show location of all known and/or potential HW sites identified.

1. Project Features: New R/W? No Excavation? Yes Railroad Involvement? No

Structure demolition/modification? No Subsurface utility relocation? No

Initial Site Assessment (ISA) Checklist

(continued)

2. Project Setting: The project alignment will travel through highly urbanized areas of Oceanside. The Class III bike route along Pier View Way, Pacific Street, and Tyson Street will be aligned through a generally residential neighborhood on existing City streets. The paved streets will not be modified and will remain in their existing condition. Between Tyson Street and Oceanside Boulevard the Class I bike path will travel adjacent to single-family and multifamily residential development, as well as some commercial uses. This area has been totally disturbed. There are several trees within the alignment. Most properties lining the railroad right-of-way have fencing separating the property from the railroad tracks.

Rural or Urban Urban
Current land uses NCTD and City right-of-way
Adjacent land uses General commercial, residential, NCTD and City rights-of-way; and transportation corridor

3. Check federal, state, and local environmental and health regulatory agency records as necessary, to see if any known hazardous waste site is in or near the project area. If a known site is identified, show its location on the attached map and attach additional sheets, as needed, to provide pertinent information for the proposed project.

4. Conduct Field Inspection. Date January 7, 2003. Use the attached map to locate potential or known HW sites.

STORAGE STRUCTURES / PIPELINES:

| | | | |
|-------------------|-------------|---------------|-------------|
| Underground tanks | <u>None</u> | Surface tanks | <u>None</u> |
| Sumps | <u>None</u> | Ponds | <u>None</u> |
| Drums | <u>None</u> | Basins | <u>None</u> |
| Transformers | <u>None</u> | Landfill | <u>None</u> |
| Other | <u>None</u> | | |

CONTAMINATION: (spills, leaks, illegal dumping, etc.)

| | | | |
|------------------|-------------|-------------------|-------------|
| Surface staining | <u>None</u> | Oil sheen | <u>None</u> |
| Odors | <u>None</u> | Vegetation damage | <u>None</u> |
| Other | <u>None</u> | | |

HAZARDOUS MATERIALS: (asbestos, lead, etc.)

| | | | |
|--------------------|-------------|-----------------------|-------------|
| Buildings | <u>None</u> | Spray-on fireproofing | <u>None</u> |
| Pipe wrap | <u>None</u> | Friable tile | <u>None</u> |
| Acoustical plaster | <u>None</u> | Serpentine | <u>None</u> |
| Paint | <u>None</u> | Other | <u>None</u> |

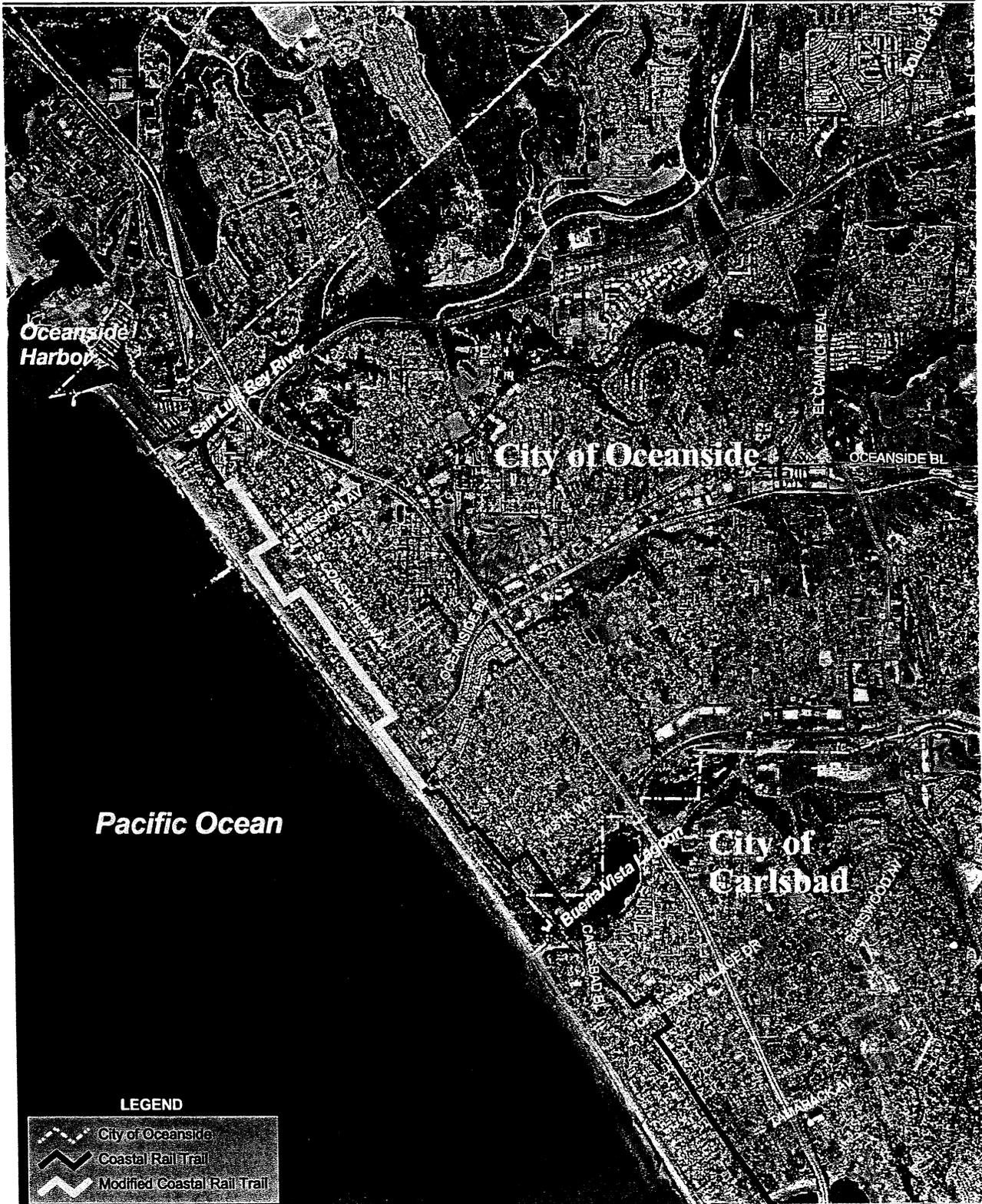


Figure 1
Regional Map

Initial Site Assessment (ISA) Checklist

(continued)

5. Additional record search, as necessary, of subsequent land uses that could have resulted in a hazardous waste site. Use the attached map to show the location of potential hazardous waste sites.

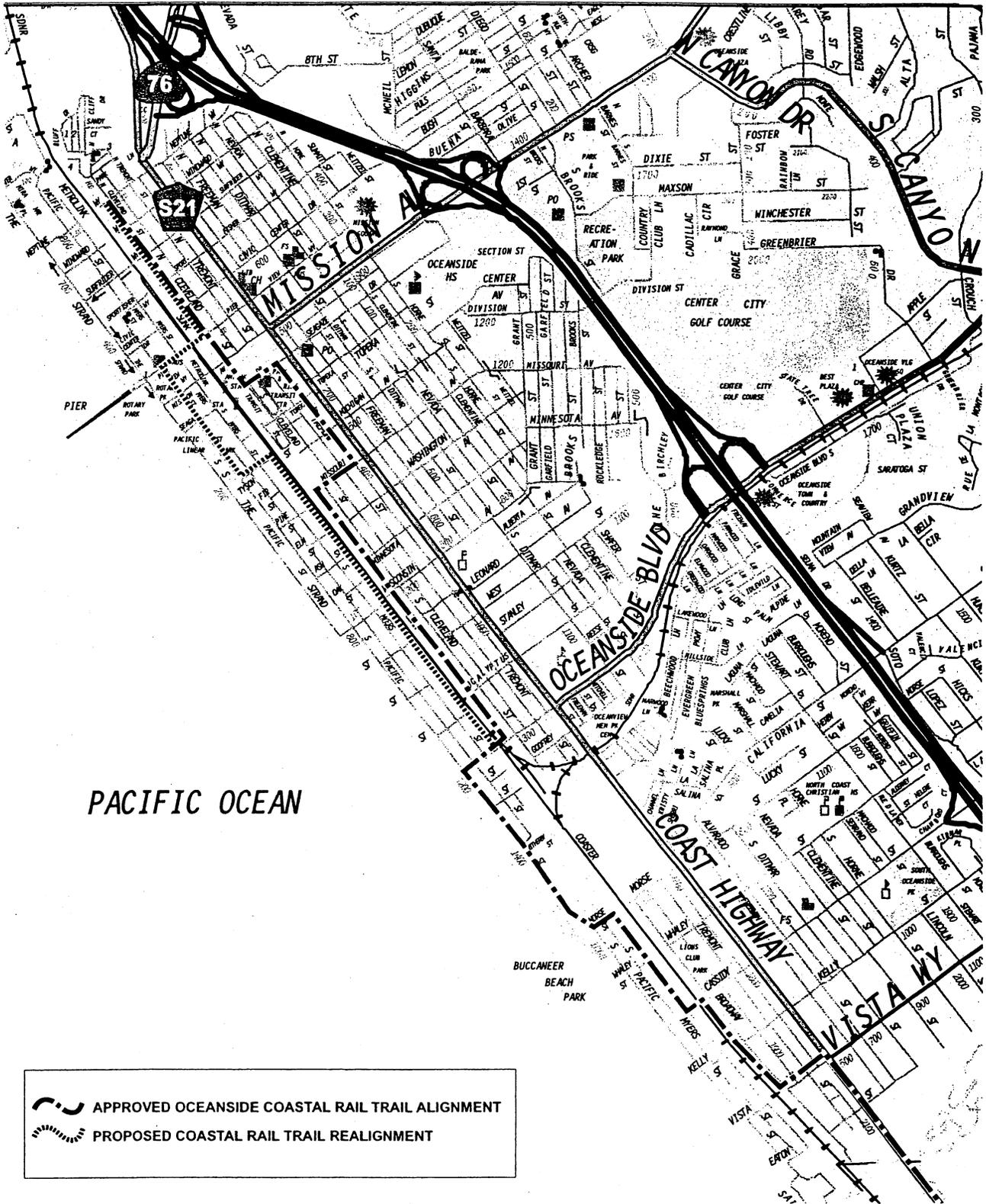
6. Other comments and/or observations: See attached maps and supplemental discussion.

ISA Determination

Does the project have potential hazardous waste involvement? No If there is known or potential hazardous waste involvement, is additional ISA work needed before task orders can be prepared for the Investigation? No
If "YES," explain; then give an estimate of additional time required:

A brief memo should be prepared to transmit the ISA conclusions to the Project Manager and Project Engineer.

ISA Conducted by EDAW, Inc. Date February 2003



PACIFIC OCEAN

 APPROVED OCEANSIDE COASTAL RAIL TRAIL ALIGNMENT
 PROPOSED COASTAL RAIL TRAIL REALIGNMENT

Source: Thomas Bros. 2002



Figure 2
Vicinity Map

Oceanside Coastal Rail Trail

1K219 Coastal Rail Trail\Figures\Fig 2 Vicinity Map04 (bradyd) 1/17/03

SUPPLEMENTAL ISA CHECKLIST DISCUSSION

SURVEY SCOPE

The purpose of this report is to identify potential hazardous waste impacts associated with implementation of the northern portion of the Oceanside coastal rail trail project. The Oceanside Coastal Rail Trail project has an approved Mitigated Negative Declaration; however, a segment of the northern portion of the trail has been redesigned. This study was conducted to determine whether the realigned portion of the project could be affected by any recorded or visible hazardous waste problems, and to recommend any additional ISA work (such as review of site records or historical aerial photographs) that may be needed prior to completion of the draft environmental document for the realigned portion of the project. Subsurface investigation, detailed geologic mapping, laboratory analysis of soil or groundwater samples, and an aerial photography review were not part of this investigation. A summary of the survey scope is provided below.

EDAW, Inc. (EDAW) acquired an agency records search for the project vicinity to identify all hazardous waste sites classified as such under state law. The records search identified business types located within the project study area likely to store, transfer, or utilize large quantities of hazardous materials. This information was obtained from records maintained by the State of California Department of Health, the Regional Water Quality Control Board, and other appropriate agencies. EDAW utilized the environmental services of Environmental Data Resources, Inc. (EDR) to perform this search.

GOVERNMENTAL RECORDS SEARCH

EDAW consulted with EDR to obtain government records of any known hazardous waste sites, hazardous waste incidents, or hazardous waste generators within a 1.6-kilometer (1-mile) radius of the Oceanside Coastal Rail Trail. The records search included a number of agency database sources to determine whether any known hazardous waste sites exist in the vicinity of the project. The records search was conducted in accordance with the American Society for Testing and Materials (ASTM) Real Estate Transaction Screen (ASTM 1528-96). The following is a list of agencies, databases, and types of records consulted by EDR in its governmental records search:

Map Findings Summary

A) Federal ASTM Standard:

| | | |
|----------|--------------------------|--|
| U.S. EPA | NPL | National Priority List |
| U.S. EPA | Proposed NPL | Proposed National Priority List Sites |
| U.S. EPA | CERCLIS | Comprehensive Environmental Response, Compensation, and Liability Information System |
| U.S. EPA | CERC-NFRAP | CERCLIS- No Further Remedial Action Planned |
| U.S. EPA | CORRACTS | Corrective Actions Report |
| U.S. EPA | RCRIS-TSD | Resource Conservation and Recovery Information System |
| | RCRIS/ Lg. Quan. Gen. | Resource Conservation and Recovery Information System |
| | RCRIS/ Sm. Quan. Gen | Resource Conservation and Recovery Information System |
| | ERNS | Emergency Response Notification System |

B) State ASTM Standard:

| | | |
|--------|-------------------|---|
| | AWP | Annual Workplan Sites |
| CA TSC | Cal-Sites | Database that contains both known and potential hazardous substance sites. |
| CA OES | CHMIRS | California Hazardous Material Incident Report System |
| CA EPA | Cortese | Database that identifies public drinking water wells with detectible levels of contamination. |
| SWRCB | Notify 65 | Facility notification about any release that could impact drinking water. |
| | Toxic Pits | Toxic Pits Cleanup Act Sites |
| STATE | State Landfill | Closed and Inactive Landfills |
| SWRCB | WMUDS/SWAT | Waste Management Unit Database |
| SWRCB | LUST | Leaking Underground Storage Tanks Information System |
| | CA Bond Exp. Plan | Bond Expenditure Plan |
| SWRCB | UST | Active Underground Storage Tank Facilities |
| DTSC | VCP | Contains low threat level properties |
| CA EPA | CA FID UST | Facility Inventory Database |
| SWRCB | HIST UST | Hazardous Substance Storage Container Database |

C) Federal ASTM Supplemental:

| | | |
|---------------|--------------|--|
| | CONSENT | Superfund (CERCLA) Consent Decrees |
| | ROD | Record of Decision |
| | Delisted NPL | National Priority List Deletions |
| U.S. EPA/NTIS | FINDS | |
| | HMIRS | Hazardous Materials Information Reporting System |
| | MLTS | Material Licensing Tracking System |
| | MINES | Mines Master Index File |
| | NPL Liens | Federal Superfund Liens |
| | PADS | PCB Activity Database System |

| | |
|-------|---|
| RAATS | RCRA Administrative Action Tracking System |
| TRIS | Toxic Chemical Release Inventory System |
| TSCA | Toxic Substances Control Act |
| SSTS | Section 7 Tracking Systems |
| FTTS | FIFRA(Federal Insecticide, Fungicide, & Rodenticide Act)/ TSCA (Toxic Substances Control Act) Tacking System |

D) State or Local ASTM Supplemental:

| | | |
|--------|--------------------|--|
| | AST | Aboveground Petroleum Storage Tank Facilities |
| DTSC | CLEANERS | Cleaner Facilities |
| | CA WDS | Waste Discharge System |
| | DEED | List of Deed Restrictions |
| | CA SLIC | Spills, Leaks, Investigation & Cleanup Cost Recovery Listing |
| DTSC | HAZNET | Hazardous Waste Information System |
| HMMD | San Diego Co. HMMD | San Diego County Hazardous Materials Management Division Database |
| DLMSHA | MINES | Mines Master Index File |

E) EDR Proprietary Historical Databases:

| | | |
|-----|----------|-------------------------------|
| RPS | Coal Gas | Former Manufactured Gas Sites |
|-----|----------|-------------------------------|

Records Search Results

The EDR database search did not identify any hazardous materials, hazardous spills, or landfills on or directly adjacent to the project site. State and federal databases searched as part of the EDR's records review identified 32 sites within a 1.6-kilometer (1-mile) radius of the project site. None of these sites was located along the realigned portion of the trail.

An additional 34 sites were listed on an orphan site list. Orphan sites generally do not provide the entire address of the hazardous substance site. Seven of the sites did not provide complete and/or accurate addresses and their locations were unidentified. None of the identifiable orphan sites were located on the proposed trail route.

VISUAL SITE SURVEY

The visual site survey of the proposed project was conducted by EDAW personnel on January 7, 2003. The on-site survey consisted of visually inspecting the areas along and adjacent to the proposed trail alignment. No visible evidence was observed of any significant spillage or significant surface staining near the proposed route.

The vegetation located along the site was healthy at the time of the survey. A small amount of debris items such as plastic bottles, paper cups, aluminum cans, and broken glass were observed along the proposed route.

Land uses adjacent to the trail consist of NCTD and City right-of-way, residential, commercial, and transportation corridor (railroad).

CONCLUSIONS

Government Records Search

No known incidences of spillage or illegal dumping have been recorded along the proposed route. The governmental records search conducted for the proposed project identifies 32 sites within 1.6 kilometers (1 mile) of the proposed realignment. None of these sites is located adjacent to the proposed route. No National Priorities List (NPL) sites, which have the greatest potential for widespread regional contamination, were identified within the 1.6-kilometer (1-mile) radius searched. Although no major hazardous spill incidents are recorded as occurring on the project site, this does not rule out the possibility of unrecorded, illegal dumping activities.

Visual Site Survey

No additional hazardous concerns were identified during the visual site survey. However, it was noted during the survey that underground infrastructures, such as sewer, electric, and water facilities, were located north of Morse Street on the southern embankment of Buccaneer Beach Park.

RECOMMENDATIONS FOR FURTHER WORK

Since the proposed project would not involve the demolition of any buildings, an asbestos survey by the California Department of Transportation (Caltrans) Hazardous Waste Section would not be required. No additional research is anticipated for the proposed trail alignment.

LIMITATIONS

The ISA was performed in accordance with the Caltrans guidelines for ISAs. During the performance of the assessment, as much data as possible pertaining to the project site were collected and reviewed. This assessment should not be construed as a full-scale environmental site investigation to prove that the project site is environmentally devoid of hazardous or toxic materials. Information and data were provided by presumably competent third parties with knowledge about the site and surrounding areas. The presence of radioactive materials, asbestos-containing materials, and biological hazards was not specifically investigated.

EDAW's services consist of professional opinions and recommendations made in accordance with generally accepted environmental principles and practices. The conclusions are based upon an evaluation of the information gathered and general observations of conditions prevalent at the project site during the site visit. EDAW does not otherwise provide any implied or expressed guarantees regarding the characteristics or conditions of environmental media at the project site.

ATTACHMENT D
COASTAL CONSISTENCY DETERMINATION

**COASTAL CONSISTENCY DETERMINATION
COASTAL RAIL TRAIL PROJECT PHASE 2
OCEANSIDE, CALIFORNIA
May 23, 2005**

1. AUTHORITY

This Coastal Consistency Determination (CCD) is submitted in compliance with 15 CFR Section 930.34 *et seq* of the National Oceanic and Atmospheric Administration (NOAA) Federal Consistency Regulations (15 CFR 930).

2. DETERMINATION

The Oceanside Coastal Rail Trail (Phase 2) project is located in Oceanside, California (see figures 1 and 2 of the Mitigated Negative Declaration (MND) for a vicinity and location map). The following CCD has been prepared to determine whether the project is consistent with the City of Oceanside Local Coastal Program (LCP) and the California Coastal Act (Public Resources Code, Division 20). The policies presented in the LCP are based on the policies of the California Coastal Act (CCA). The LCP was certified by the California Coastal Commission in July 1985. This CCD presents the general articles in Chapter 3 of the CCA, followed by comments describing the project's consistency with these articles. Project consistency with the LCP and CCA will insure consistency with the Federal Coastal Zone Management Act. The Coastal Rail Trail (Phase 2) project was found to be consistent to the maximum extent practicable with the California Coastal Act of 1976 as amended. The MND and associated studies for this project provide the basis for this finding.

3. PROJECT DESCRIPTION

The City of Oceanside (City) proposes to construct a segment of the Coastal Rail Trail, a 71-kilometer-long (44-mile-long) regional bicycle and pedestrian pathway/trail that will extend from Oceanside to downtown San Diego. The alignment of the Coastal Rail Trail in Oceanside was evaluated in the approved *Final Mitigated Negative Declaration, Coastal Rail Trail Project, Oceanside to Del Mar*. Subsequently, two minor modifications to the Oceanside alignment have been made. The southern modification is located generally between South Myers Street and Vista Way. This has been evaluated in a separate MND and has been constructed.

The subject of this CCD is a modification to the northern segment, generally between the San Luis Rey trail connection and Oceanside Boulevard. In the 2001 Final MND, the segment between Tyson Street and Oceanside Boulevard consisted of a Class I bike path on the east side of the NCTD railroad tracks. NCTD now proposes to build an additional railroad track in this location, so the bike path is proposed to be relocated to the west side. To route bicycle traffic to the west side, a portion of the Class III bike route along Cleveland Street, Neptune Way, Pier View Way and Pacific Street will be modified as well. More detailed information about the original project and modifications are provided in the Project Description of the MND.

The northern segment of the Oceanside Coastal Rail Trail (Phase 2) will begin as a Class III bike route at Cleveland Street and Neptune Way (see Figure 2 of the MND). The Class III bike route will continue on Cleveland Street and Pier View Way. At Pier View Way, the Class III bike route will turn to the west, cross under the Pier View Way pedestrian undercrossing, and continue to Pacific Street. The route will then head south on Pacific Street. At Tyson Street, the route will turn to the east and travel along Tyson Street until reaching the connection with the Class I bike path. Signs for the Class III bike route will be mounted on existing light poles or sign posts. No permanent improvements or roadway striping is proposed along the Class III bike route.

The Class I bike path will head south within the City right-of-way from Tyson Street to Wisconsin Avenue on the west side of the railroad tracks. Just north of Wisconsin, the trail will be located on the east side of an existing City pay parking lot. The parking lot will be restriped and one additional parking space will be created. The trail will continue south and cross Wisconsin Avenue in a marked crosswalk adjacent to the parking lot entrance. South of Wisconsin Avenue, the trail will be located within the North County Transit District (NCTD) right-of-way, between adjacent residences and the existing railroad tracks. As part of the project, a 6-foot, 4-inch welded wire fence would be constructed between the bike path and the railroad to prevent unauthorized access. Existing private fences will be protected in place on the west side of the path. The bike path will continue south in the NCTD right-of-way to Oceanside Boulevard where there will also be a marked crosswalk for safe bike crossing. A Class III bike route will then continue west on Oceanside Boulevard and south on Pacific Street, where it will eventually connect to the southern Oceanside segment of the trail.

4. CONSISTENCY WITH PROVISIONS OF THE CALIFORNIA COASTAL ACT

ARTICLE 2, PUBLIC ACCESS

One of the objectives of the Coastal Rail Trail is to provide increased access to coastal areas. The proposed trail alignment is located parallel to the coast, typically within less than four blocks of the beach. Trail users would be able to exit the trail and use city streets to access the coast.

The trail would connect to the southern portion of the Oceanside Coastal Rail Trail and to the San Luis Rey Recreational Trail to the north, thus providing a through link for alternative modes of transportation to access along the coastline throughout Oceanside.

The trail alignment would necessitate the restriping of a city parking lot; however, no parking spaces would be lost. In addition, the trail promotes alternative means of transportation along the coast and parking availability near coastal areas would not be impacted by this project. The project would be consistent with Article 2 of the CCA.

ARTICLE 3, RECREATION

The Coastal Rail Trail will provide a new coastal recreational opportunity in the form of a pedestrian/bicycle trail. The Class III bike route will pass near three community parks along

Pacific Street and will be near the beach. This trail will provide users with increased access to local and regional parks including the beach.

ARTICLE 4, MARINE ENVIRONMENT

The Coastal Rail Trail would generally be located 4 blocks from the coast, with one short segment located approximately 1 block from the beach. As outlined in the MND, best management practices and infiltration and sediment retention features would be implemented during construction activities. As identified in the MND, potential runoff impacts would be minimal with no adverse impact to the marine environment. The project would be consistent with Article 4 of the CCA.

As required by the City of Oceanside, a Standard Urban Stormwater Mitigation Plan will be prepared to document the effectiveness of project drainage/runoff control features. During operation of the project, the marine environment would not be affected by use of the trail or by the newly created impervious surface. The project would be consistent with this article concerning marine environment.

ARTICLE 5, LAND RESOURCES

The Coastal Rail Trail would not impact important or valuable land resources near the coast. A portion of the trail alignment would be located on City streets and would require only restriping of the pavement. The remaining portion would be located within the NCTD railroad right-of-way. This land is currently vacant and is not available for public use due to safety concerns. As outlined in the MND, no valuable land resources, such as biological habitat, would be significantly impacted and the project would be consistent with this article.

ARTICLE 6, DEVELOPMENT

The Coastal Rail Trail would be consistent with Article 6 of the CCA and would enhance public access to the coast. The trail would not obstruct any views and may provide recreationalists with views of the coast at certain locations. The project would provide a connected north/south access route for alternative modes of transportation, such as walking or biking, to coastal areas throughout Oceanside. The trail would not adversely impact parking availability at the coast and potentially reduce demand on local parking because it would encourage people to walk or bike instead of drive to the beach area, and restriping the existing parking lot would increase the total number of spaces available.

ARTICLE 7, INDUSTRIAL DEVELOPMENT

The project proposes no industrial development and would not impact existing industrial development, thus, this article does not apply to the Coastal Rail Trail project.

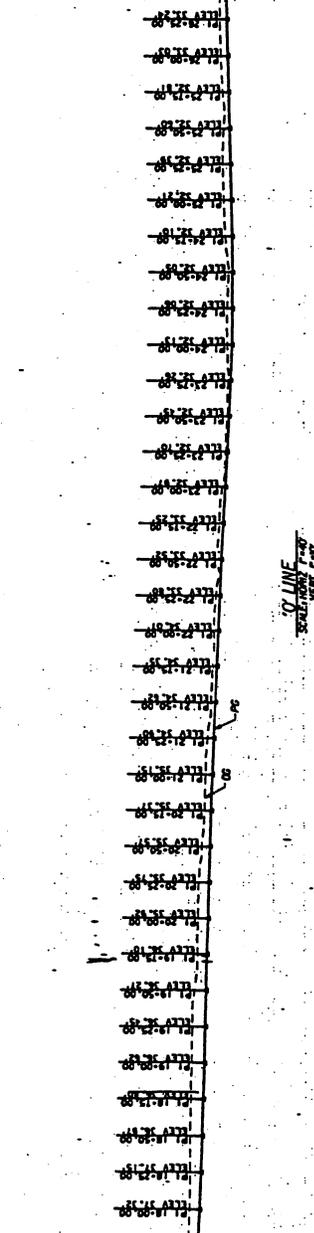
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MATCH LINE - SEE SHEET 3

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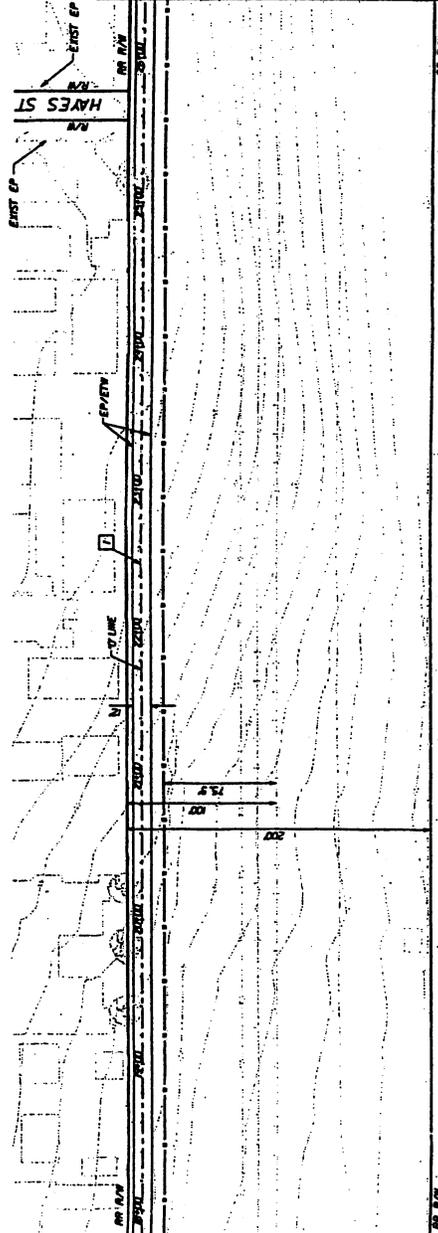
MATCH LINE - SEE SHEET 5

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50
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CITY CENTERLINE
VERT. 1-07

18+00 19+00 20+00 21+00 22+00 23+00 24+00 25+00



MATCH LINE - SEE SHEET 3

MATCH LINE - SEE SHEET 5

| CITY CENTERLINE DATA | | | |
|----------------------|---|-----------|--------|
| IN | R | Δ | BLK. # |
| 111 | — | 137-4250W | BLK. 7 |

LEGEND

- PROPOSED FILL
- PROPOSED CONC
- PROPOSED FINISH
- PROPOSED BASE
- PROPOSED STAMPED CONCRETE
- EXISTING GAS
- EXISTING ELECTRIC
- EXISTING WATER
- EXISTING TELEPHONE
- EXISTING FIBER OPTIC
- EXISTING STORM DRAIN
- STATION LINE
- 0+00
- WELDED WIRE FENCE PER CONST DETAIL SHY 1'
- RR R/W
- DRAINAGE DITCH
- CITY R/W

PLAN SHEET 70

- NOTES:
- FOR DRAINAGE IMPROVEMENTS SEE GRADING AND DRAINAGE PLANS ON SHEETS 8-10.



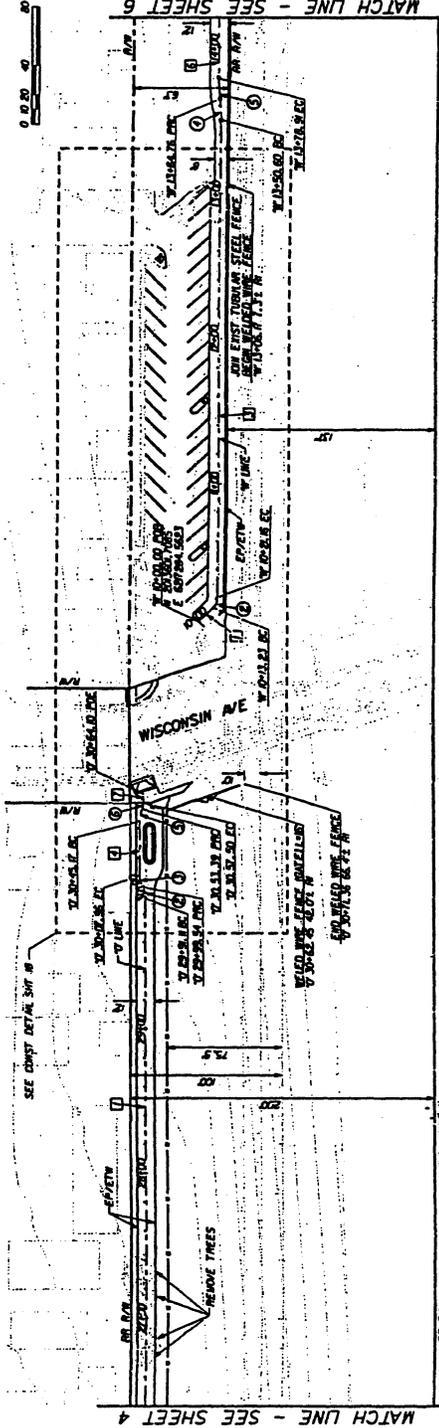
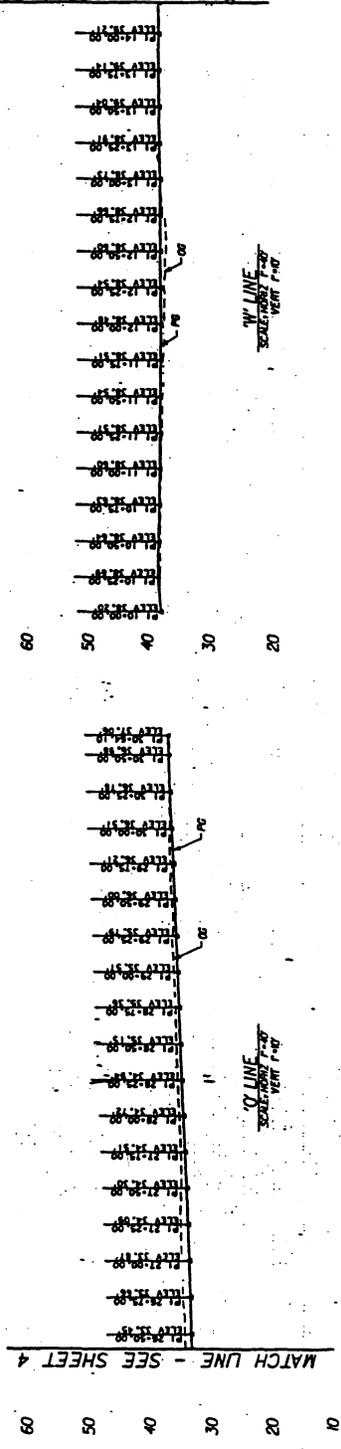
DEDOKKEN
ENGINEERING, INC.
10000
STATE OF FLORIDA
LICENSE NO. 10000

70X SUBMITTAL - NOT FOR CONSTRUCTION
OCTOBER 15, 2004

DESCRIPTION: IMPROVEMENT PLANS FOR
COASTAL DIAL TRAIL - PHASE 2
CITY CENTERLINE DATA

| | |
|---|----------------------------|
| CITY OF OCEANSIDE IMPROVEMENT DISTRICT | 34 SHEET |
| PROJECT NO. 04-001 | DATE 10/15/04 |
| DESIGNED BY: J. J. DEDOKKEN | CHECKED BY: J. J. DEDOKKEN |
| DRAWN BY: J. J. DEDOKKEN | DATE: 10/15/04 |
| SCALE: 1" = 40' | PROJECT: 04-001 |
| DATE: 10/15/04 | PROJECT: 04-001 |

MATCH LINE - SEE SHEET 6



LEGEND

- PROPOSED FALL
- COMPACTED
- PROPOSED PCC
- PROPOSED CRUSHED AGGREGATE BASE
- STAMPED CONCRETE
- EXISTING GAS
- EXISTING ELECTRIC
- EXISTING WATER
- EXISTING TELEPHONE
- EXISTING SEWER
- EXISTING FIBER OPTIC
- EXISTING STORM DRAIN
- STATION LINE
- 10'00'
- WELDED WIRE FENCE
- PER-CONST DETAIL SHT. #
- DRAINAGE DITCH
- A/R A/W
- CITY A/W

PLAN
SCALE: 1"=40'

NOTES:

① FOR DRAINAGE IMPROVEMENTS SEE GRADING AND DRAINAGE PLANS ON SHEETS B-12

70% SUBMITTAL - NOT FOR CONSTRUCTION
OCTOBER 15, 2004

DATE: THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE CITY OF OCAWASCO'S STANDARD SPECIFICATIONS FOR ROADWAY CONSTRUCTION, LATEST EDITION, AND TO THE CITY OF OCAWASCO'S STANDARD SPECIFICATIONS FOR UTILITIES, LATEST EDITION.

DESCRIPTION: IMPROVEMENTS TO WISCONSIN AVE FROM STA 14+00 TO STA 27+50. THIS PROJECT INCLUDES: 1. RECONSTRUCTION OF EXISTING PAVED SURFACE TO 12' WIDE. 2. RECONSTRUCTION OF EXISTING SIDEWALKS TO 6' WIDE. 3. RECONSTRUCTION OF EXISTING CURBS TO 4" HIGH. 4. RECONSTRUCTION OF EXISTING STORM DRAINAGE SYSTEM. 5. RECONSTRUCTION OF EXISTING UTILITY LINES. 6. RECONSTRUCTION OF EXISTING WELDED WIRE FENCE. 7. RECONSTRUCTION OF EXISTING TREES. 8. RECONSTRUCTION OF EXISTING LIGHT FIXTURES. 9. RECONSTRUCTION OF EXISTING SIGNAGE. 10. RECONSTRUCTION OF EXISTING STREET LIGHTS. 11. RECONSTRUCTION OF EXISTING STREET LIGHT POLES. 12. RECONSTRUCTION OF EXISTING STREET LIGHT WIRING. 13. RECONSTRUCTION OF EXISTING STREET LIGHT CONTROLS. 14. RECONSTRUCTION OF EXISTING STREET LIGHT FOUNDATIONS. 15. RECONSTRUCTION OF EXISTING STREET LIGHT FOUNDATION WALLS. 16. RECONSTRUCTION OF EXISTING STREET LIGHT FOUNDATION SLABS. 17. RECONSTRUCTION OF EXISTING STREET LIGHT FOUNDATION BEAMS. 18. 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ENGINEERS
INCORPORATED
1000 W. WISCONSIN AVE
OCAWASCO, IL 60143
TEL: 815.486.1234
FAX: 815.486.1235
WWW.DEDOKKEN.COM

CITY OF OCAWASCO
IMPROVEMENT PLANS FOR
WISCONSIN AVE FROM STA 14+00 TO STA 27+50

DATE: 10/15/04
DRAWN BY: [Name]
CHECKED BY: [Name]
APPROVED BY: [Name]

SCALE: 1"=40'

SHEET NO. 34

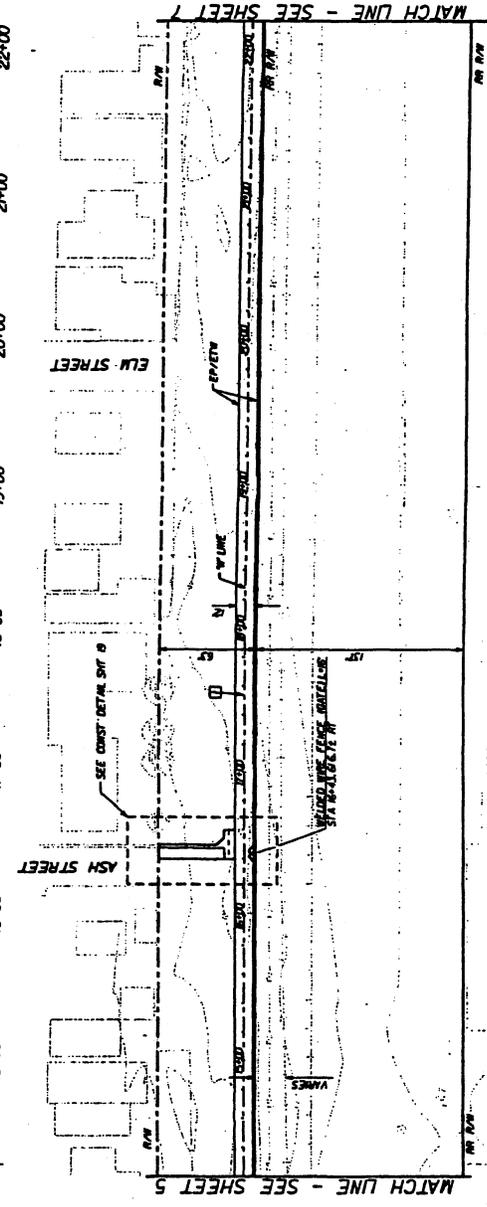
MATCH LINE - SEE SHEET 5

60
50
40
30
20

15+00 16+00 17+00 18+00 19+00 20+00 21+00 22+00

MATCH LINE - SEE SHEET 7

60
50
40
30
20



| STATION | W LINE CENTERLINE DATA |
|---------|------------------------|
| 15+00 | 834.00 |
| 16+00 | 834.00 |
| 17+00 | 834.00 |
| 18+00 | 834.00 |
| 19+00 | 834.00 |
| 20+00 | 834.00 |
| 21+00 | 834.00 |
| 22+00 | 834.00 |

PLAN SCALE 1"=20'

NOTES:

- 1 FOR DRAINAGE IMPROVEMENTS SEE DRAINING AND DRAINAGE PLANS ON SHEETS 6-10.

- EXISTING GAS
- EXISTING WATER
- EXISTING TELEPHONE
- EXISTING FIBER OPTIC
- EXISTING STORM DRAIN
- STATION LINE
- WELDED WIRE FENCE PER CONST DETAIL SHF 17
- DRAINAGE DITCH
- RAILROAD TRACKS
- CITY ROW

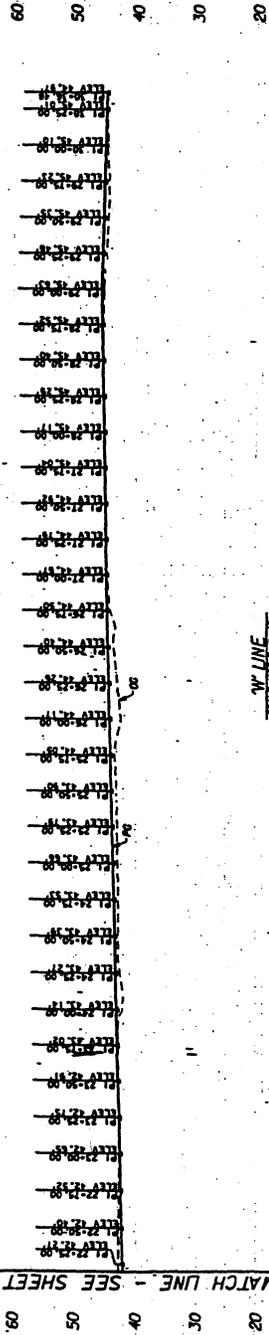
LEGEND

- PROPOSED CRUSHED FILL
- PROPOSED ASPHALT
- PROPOSED PCC CONCRETE
- PROPOSED CRUSHED AGGREGATE BASE
- STAMPED CONCRETE

FOR SUBMITTAL - NOT FOR CONSTRUCTION
OCTOBER 15, 2004

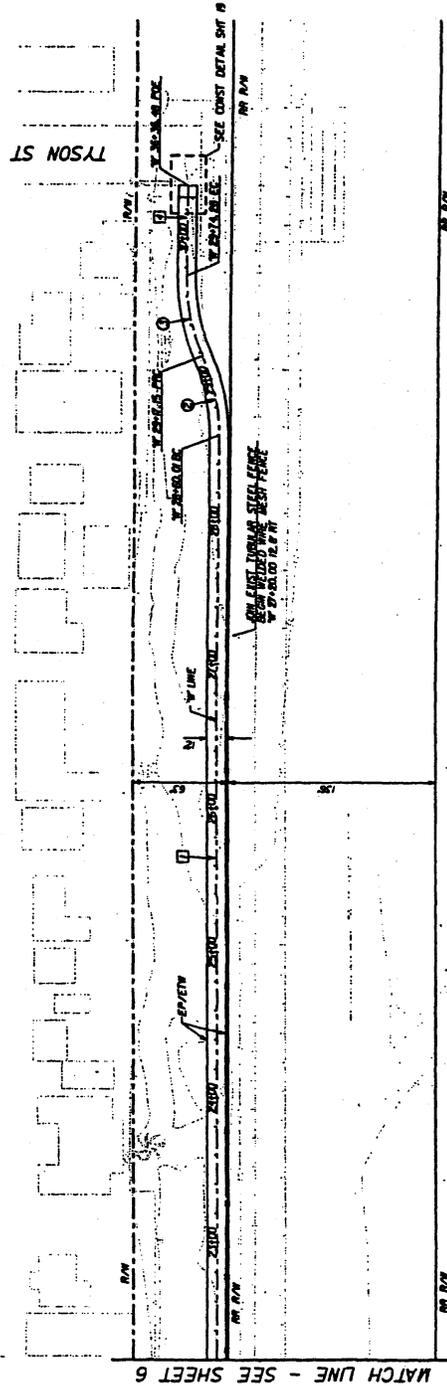
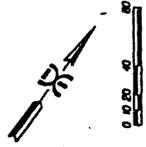
DATE PLOTTED: 3/15/2005
PROJECT: IMPROVEMENT PLANS FOR COASTAL RAIL TRAIL - PHASE 2
SHEET: 6
CITY OF OCEANOBE
DESIGNED BY: [Name]
CHECKED BY: [Name]
DATE: [Date]

DEDOKKEN
ENGINEERS & ARCHITECTS
2200 N. OCEAN BLVD., SUITE 100
OCEANOBE, CA 94947
TEL: (415) 435-1000
FAX: (415) 435-1001
WWW.DEDOKKEN.COM



W LINE
 SOUTH BAY F-30
 NEW F-30

| W LINE CENTERLINE DATA | | | |
|------------------------|--------------|--------|--------|
| STATION | Δ | L | Δ |
| 23+00 | 135° 46' 07" | 488.07 | 488.07 |
| 24+00 | 135° 46' 07" | 57.13 | 57.13 |
| 25+00 | 135° 46' 07" | 57.13 | 57.13 |
| 26+00 | 135° 46' 07" | 57.13 | 57.13 |
| 27+00 | 135° 46' 07" | 57.13 | 57.13 |
| 28+00 | 135° 46' 07" | 57.13 | 57.13 |
| 29+00 | 135° 46' 07" | 57.13 | 57.13 |
| 30+00 | 135° 46' 07" | 57.13 | 57.13 |



PLAN
 SCALE 1"=40'

- LEGEND**
- PROPOSED FILL
 - EXISTING ELECTRIC
 - EXISTING WATER
 - EXISTING SEWER
 - EXISTING FIBER OPTIC
 - EXISTING STORM DRAIN
 - STATION LINE
 - WELDED WIRE FENCE
 - DRAINAGE DITCH
 - CITY R/W
 - PROPOSED COMPACTED FILL
 - PROPOSED PCC
 - PROPOSED CRUSHED AGGREGATE BASE
 - STAMPED CONCRETE

NOTES

FOR DRAINAGE IMPROVEMENTS SEE GRADING AND DRAINAGE PLANS ON SHEETS 8-A.

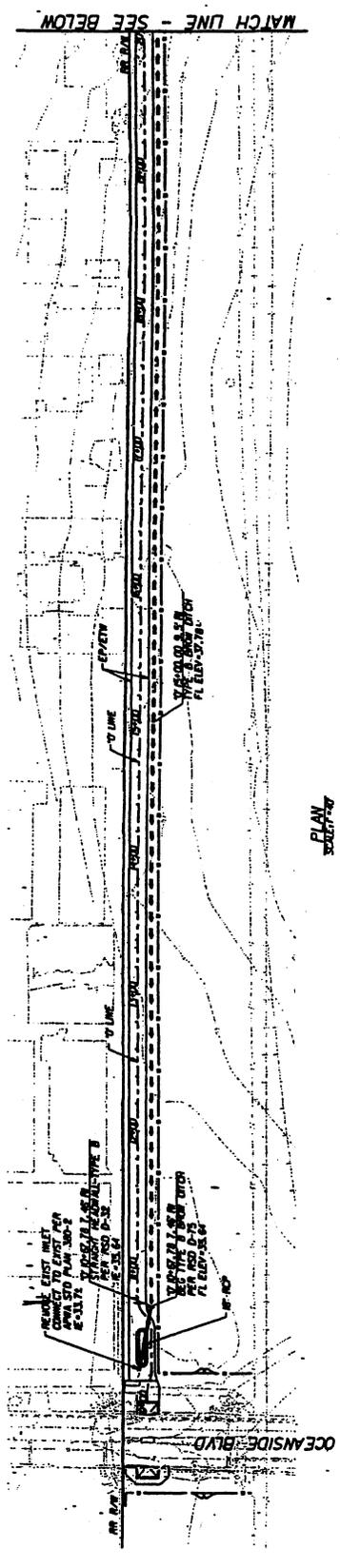
70% SUBMITTAL - NOT FOR CONSTRUCTION
 OCTOBER 15, 2004

THIS PLAN IS THE PROPERTY OF DEBOKKEN ENGINEERING AND IS TO BE USED ONLY FOR THE PROJECT AND LOCATION SPECIFICALLY IDENTIFIED HEREON. IT IS NOT TO BE REPRODUCED, COPIED, OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, INCLUDING PHOTOCOPYING, RECORDING, OR BY ANY INFORMATION STORAGE AND RETRIEVAL SYSTEM, WITHOUT THE WRITTEN PERMISSION OF DEBOKKEN ENGINEERING.

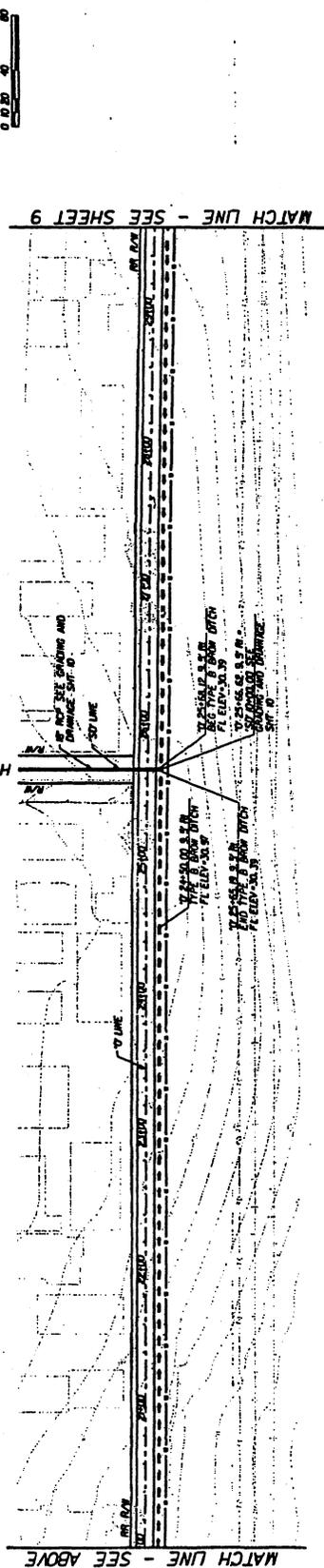


DEBOKKEN
 ENGINEERING

| | |
|----------------------------------|----------|
| PROJECT NO. | 7 |
| CITY OF OCEANSIDE | |
| IMPROVEMENT PLANS FOR | |
| COASTAL HALL TRAIL - PHASE 2 | |
| W LINE STA 22+50 TO STA 30+50.48 | |
| DATE | 10/15/04 |
| SCALE | AS SHOWN |
| PROJECT SHEET | P-XXXXX |
| DATE | 10/15/04 |



PLAN SHEET 207



PLAN SHEET 208

- LEGEND**
- COMPACTED FILL
 - COMPACTED PCC
 - CONCRETE
 - AGGREGATE BASE
 - STAMPED CONCRETE
 - EXISTING GAS
 - EXISTING ELECTRIC
 - EXISTING WATER
 - EXISTING TELEPHONE
 - EXISTING SEWER
 - EXISTING STORM DRAIN
 - STATION LINE
 - WELDED WIRE MESH FENCE PER CONST DETAIL SHT 17
 - DAMAGE DITCH
 - CITY R/W

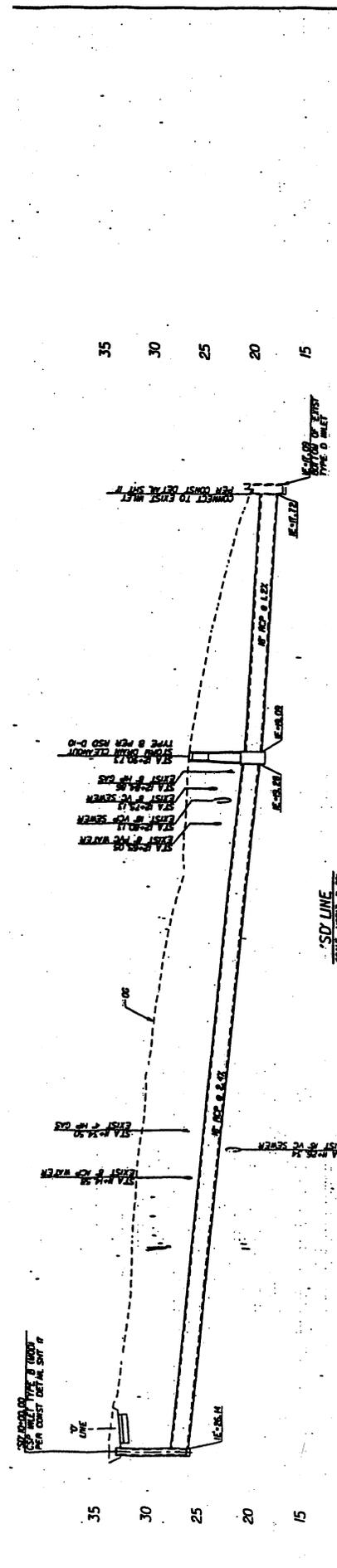
70X SUBMITTA - NOT FOR CONSTRUCTION
OCTOBER 15, 2004

DATE OF CITY ENGINEER'S ATTENTION IS INDICATED IN THE
TITLE OF THE CONTRACT DOCUMENTS. THIS DATE IS THE
DATE OF THE CITY ENGINEER'S ATTENTION TO THE CITY ENGINEER.
ALL CITY ENGINEER'S ATTENTION IS INDICATED IN THE
TITLE OF THE CONTRACT DOCUMENTS. THIS DATE IS THE
DATE OF THE CITY ENGINEER'S ATTENTION TO THE CITY ENGINEER.

| | | | |
|--------------|-------------|--|-------------|
| NO. | 0 | CITY OF OCEANSIDE ENGINEERING DEPARTMENT | 34 SHEET |
| PROJECT NAME | | GRADING AND DRAINAGE PLANS FOR COASTAL RAIL TRAIL - PHASE 2 V LINE STA 0+00 TO 29+80 | |
| DATE | 10/15/04 | PROJECT NO. | 04-0000 |
| DESIGNED BY | W. J. BROWN | CHECKED BY | R. J. BROWN |
| DRAWN BY | W. J. BROWN | APPROVED BY | R. J. BROWN |



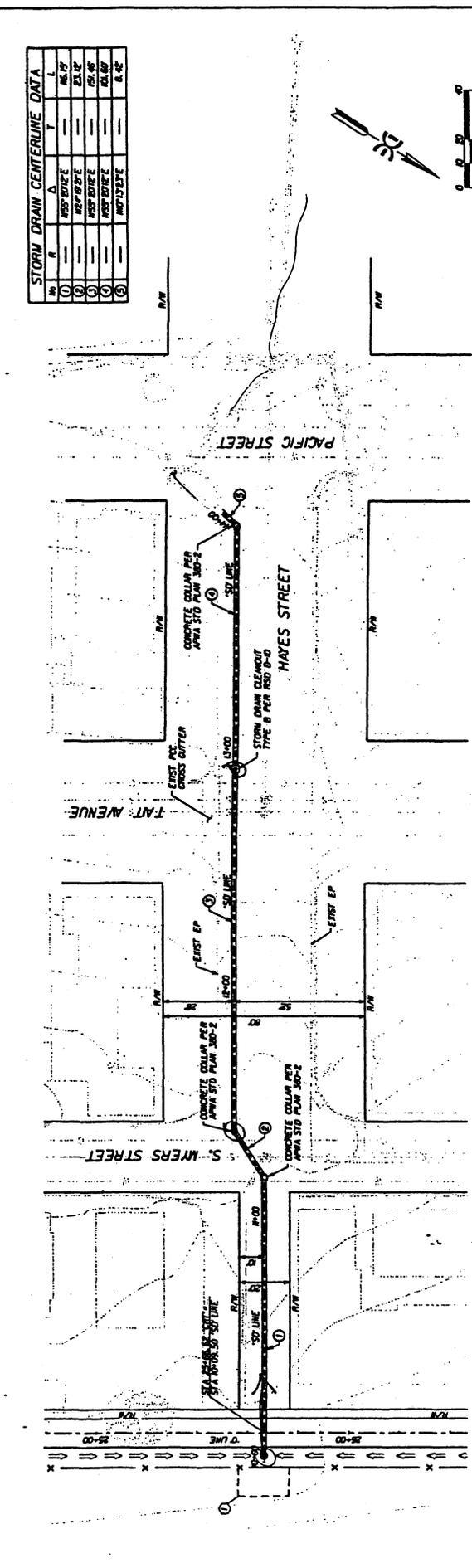
EDOKKEN
S. NOZNERINO
REGISTERED PROFESSIONAL ENGINEER
No. 10000 State of California



SD LINE
SCALE: HORIZ. 1"=30'
VERT. 1"=5'

STORM DRAIN CENTERLINE DATA

| NO. | R. | A. | T. | L. |
|-----|-------------|-----|-----|--------|
| 1 | MSW 20" I/E | --- | --- | 06.07' |
| 2 | MSW 20" I/E | --- | --- | 21.42' |
| 3 | MSW 20" I/E | --- | --- | 09.40' |
| 4 | MSW 20" I/E | --- | --- | 04.87' |
| 5 | MSW 20" I/E | --- | --- | 04.42' |



LEGEND

- PROPOSED COMPLETED FALL
- PROPOSED PCC
- PROPOSED CONCRETE
- PROPOSED APPROXIMATE WALL
- PROPOSED STAMPED CONCRETE
- EXISTING GAS
- EXISTING ELECTRIC
- EXISTING WATER
- EXISTING TELEPHONE
- EXISTING SEWER DTC
- EXISTING STORM DRAIN
- STATION LINE
- WELDED WIRE MESH FENCE PER CONST DETAIL SHIT #
- DRAINAGE DITCH
- CITY R/W

NOTES:

- LIMIT OF WORK SH. EXTEND BEYOND 25 EAST OF AIRROAD RIGHT-OF-WAY FOR ELEVATION ACCORDING TO CONSTRUCTION CSP INLET.

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OCTOBER 15, 2004

PLAN SCALE: 1"=30'

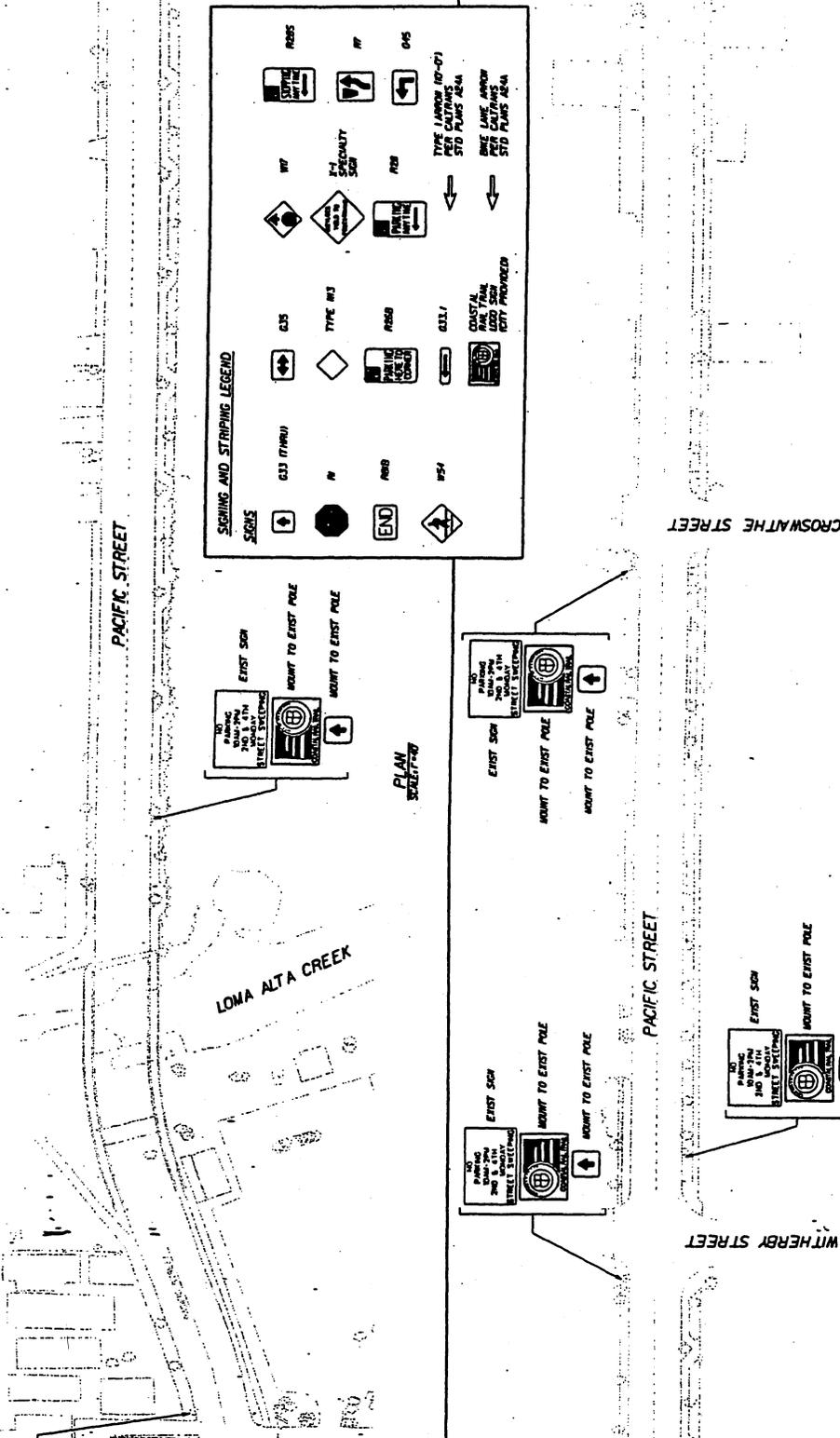
DEADOKKEN ENGINEERING
10000 S. 100th St., Suite 100
Orem, UT 84057
Phone: (801) 223-8888
Fax: (801) 223-8889
www.deadokken.com

DATE PLOT: 10/15/04 10:10:00 AM C:\Users\ADMINISTRATOR\Desktop\104417.dwg

| | | | | |
|------------------|---|----------------|------|----------------|
| PROJECT NO. | 110 | CITY OF OGDEN | DATE | 10/15/04 |
| PROJECT NAME | CHANGING AND DRAINAGE PLANS FOR CONICAL HAIL TRAIL - PHASE 2 HAYES STREET FROM HAYES STREET TO PACIFIC STREET | | | |
| PROJECT LOCATION | SOUTH OGDEN, UT | | | |
| PROJECT NUMBER | 110 | PROJECT NUMBER | 110 | PROJECT NUMBER |
| PROJECT NUMBER | 110 | PROJECT NUMBER | 110 | PROJECT NUMBER |

MATCH LINE SEE SHEET 12

MATCH LINE SEE SHEET 12



SIGNING AND STRIPING LEGEND

| | | | | |
|-------------|-----|---------|-----|------------------|
| CS3 (TRUCK) | CS5 | TYPE #3 | CS3 | TYPE JAWNS (W-7) |
| CS3 (TRUCK) | CS5 | TYPE #3 | CS3 | TYPE JAWNS (W-7) |
| CS3 (TRUCK) | CS5 | TYPE #3 | CS3 | TYPE JAWNS (W-7) |
| CS3 (TRUCK) | CS5 | TYPE #3 | CS3 | TYPE JAWNS (W-7) |
| CS3 (TRUCK) | CS5 | TYPE #3 | CS3 | TYPE JAWNS (W-7) |

COASTAL AREA TRAIL AND TRAILWAY SIGNAGE PROVIDED BY THE CITY OF SAN JOSE

PLAN 300477-20

ALL SIGNS SHOWN AS "MOUNT TO EXIST POLE" TO INDICATE CONSTRUCTION OF NEW POLE TO BE REQUIRED FOR CONSTRUCTION. NO SUCH CALL OUT REQUIRED FOR EXISTING CONSTRUCTION.

LEGEND

| | |
|---------------------------|---------------------------|
| PROPOSED CRACKED CONCRETE | PROPOSED CRACKED CONCRETE |
| PROPOSED CRACKED CONCRETE | PROPOSED CRACKED CONCRETE |
| PROPOSED CRACKED CONCRETE | PROPOSED CRACKED CONCRETE |
| PROPOSED CRACKED CONCRETE | PROPOSED CRACKED CONCRETE |
| PROPOSED CRACKED CONCRETE | PROPOSED CRACKED CONCRETE |

NOTES

- ALL SIGNS SHOWN AS "MOUNT TO EXIST POLE" TO INDICATE CONSTRUCTION OF NEW POLE TO BE REQUIRED FOR CONSTRUCTION. NO SUCH CALL OUT REQUIRED FOR EXISTING CONSTRUCTION.

DEPOKKEN
 CONSULTING ENGINEERS
 1000 SAN JOSE AVENUE
 SAN JOSE, CA 95128
 TEL: (408) 281-1111
 FAX: (408) 281-1112

| | | | | |
|-------------|--|------------------|----------------|----------------|
| DATE | 11 | CITY OF SAN JOSE | CLASSIFICATION | CLASSIFICATION |
| PROJECT | STIPING AND SIGNAGE PLANS FOR COASTAL HILL TRAIL - PHASE 2 | | | |
| PROJECT NO. | CLASS ROUTE | | | |
| DATE | 11/15/2004 | DESIGNED BY | REVIEWED BY | APPROVED BY |
| DATE | 11/15/2004 | DESIGNED BY | REVIEWED BY | APPROVED BY |

70% SUBMITTAL - NOT FOR CONSTRUCTION
 OCTOBER 15, 2004

REVISIONS

| NO. | DESCRIPTION | DATE |
|-----|-------------------|----------|
| 1 | ISSUED FOR PERMIT | 10/15/04 |

| | | |
|---------------|------|----------|
| CITY ENGINEER | DATE | 10/15/04 |
| CITY ENGINEER | DATE | 10/15/04 |
| CITY ENGINEER | DATE | 10/15/04 |

DEPOKKEN
 CONSULTING ENGINEERS
 1000 SAN JOSE AVENUE
 SAN JOSE, CA 95128
 TEL: (408) 281-1111
 FAX: (408) 281-1112

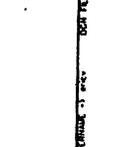
LEGEND

| | |
|---------------------------|---------------------------|
| PROPOSED CRACKED CONCRETE | PROPOSED CRACKED CONCRETE |
| PROPOSED CRACKED CONCRETE | PROPOSED CRACKED CONCRETE |
| PROPOSED CRACKED CONCRETE | PROPOSED CRACKED CONCRETE |
| PROPOSED CRACKED CONCRETE | PROPOSED CRACKED CONCRETE |
| PROPOSED CRACKED CONCRETE | PROPOSED CRACKED CONCRETE |

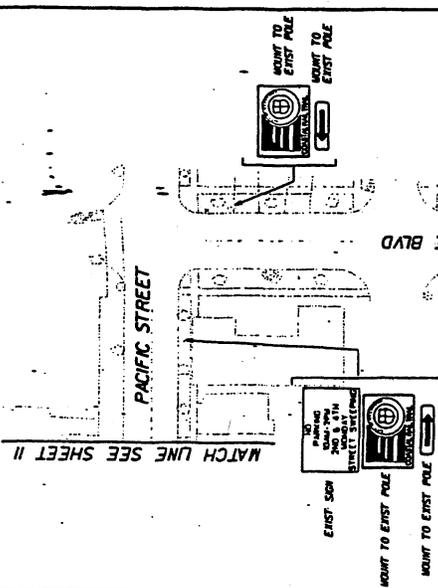
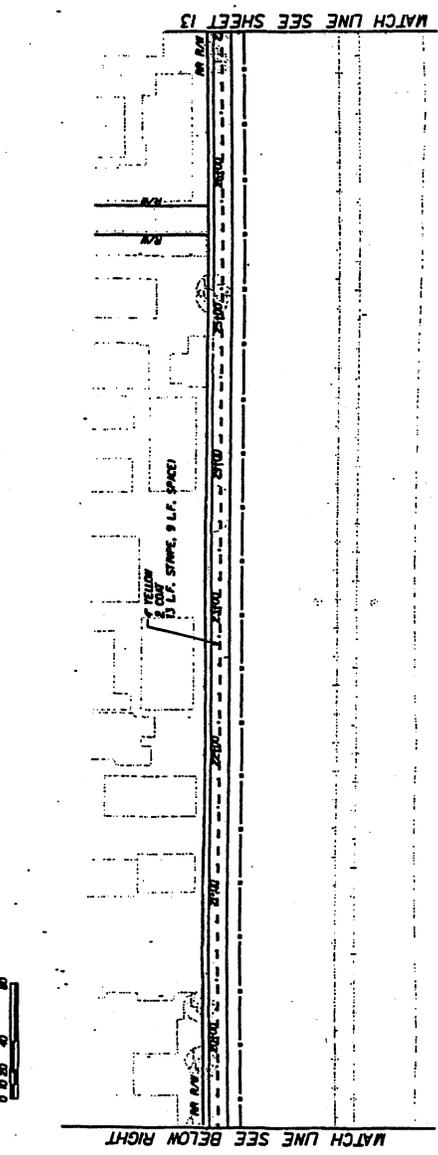
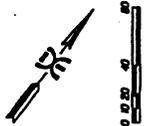
NOTES

- ALL SIGNS SHOWN AS "MOUNT TO EXIST POLE" TO INDICATE CONSTRUCTION OF NEW POLE TO BE REQUIRED FOR CONSTRUCTION. NO SUCH CALL OUT REQUIRED FOR EXISTING CONSTRUCTION.

DEPOKKEN
 CONSULTING ENGINEERS
 1000 SAN JOSE AVENUE
 SAN JOSE, CA 95128
 TEL: (408) 281-1111
 FAX: (408) 281-1112



MATCH LINE SEE ABOVE RIGHT



PLAN SCALE 1/4" = 1'-0"

PLAN SCALE 1/4" = 1'-0"

NOTES

1. ALL SLOES SHALL BE 1:1 UNLESS NOTED OTHERWISE.

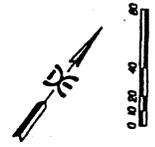
2. ALL SLOES SHALL BE 1:1 UNLESS NOTED OTHERWISE.

3. ALL SLOES SHALL BE 1:1 UNLESS NOTED OTHERWISE.

4. ALL SLOES SHALL BE 1:1 UNLESS NOTED OTHERWISE.

LEGEND

| | |
|--|---------------------------------|
| | PROPOSED FILL |
| | COMPACTED PCC |
| | CONCRETE |
| | PROPOSED CRUSHED AGGREGATE BASE |
| | STAMPED CONCRETE |
| | EXISTING GAS |
| | EXISTING ELECTRIC |
| | EXISTING TELEPHONE |
| | EXISTING SEWER |
| | EXISTING FIBER OPTIC |
| | EXISTING STORM DRAIN |
| | STATION LINE |
| | WELDED WIRE MESH FENCE |
| | PER CONST DETAIL SHT 17 |
| | DRAINAGE DITCH |
| | RR RW |
| | CITY RW |



70X SUBMITTAL - NOT FOR CONSTRUCTION
OCTOBER 15, 2004

CITY OF OCEANSIDE
ENGINEER/DESIGNER

12

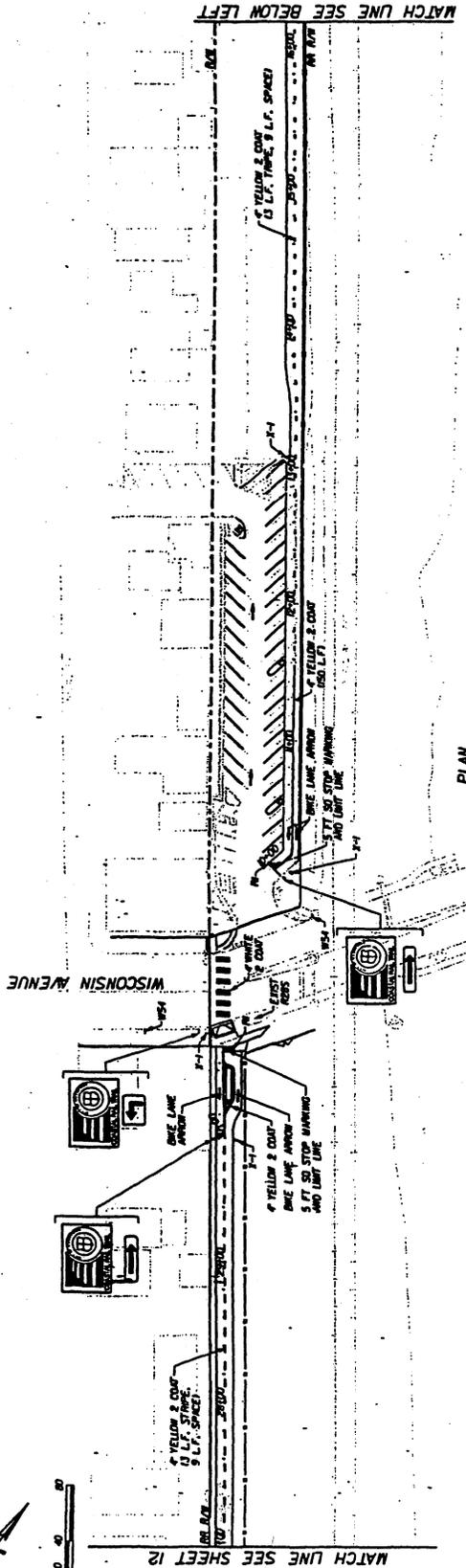
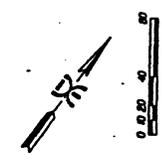
STORM AND STORAGE PIPES FOR
COASTAL HAIL TRAIL - PHASE 2
CLASS ITRAIL AND CLASS WROUTE

DATE: 10/15/04
DRAWN BY: [Name]
CHECKED BY: [Name]
PROJECT NO: [Number]
SHEET NO: 12 OF 12



DATE: 10/15/04
DRAWN BY: [Name]
CHECKED BY: [Name]
PROJECT NO: [Number]
SHEET NO: 12 OF 12

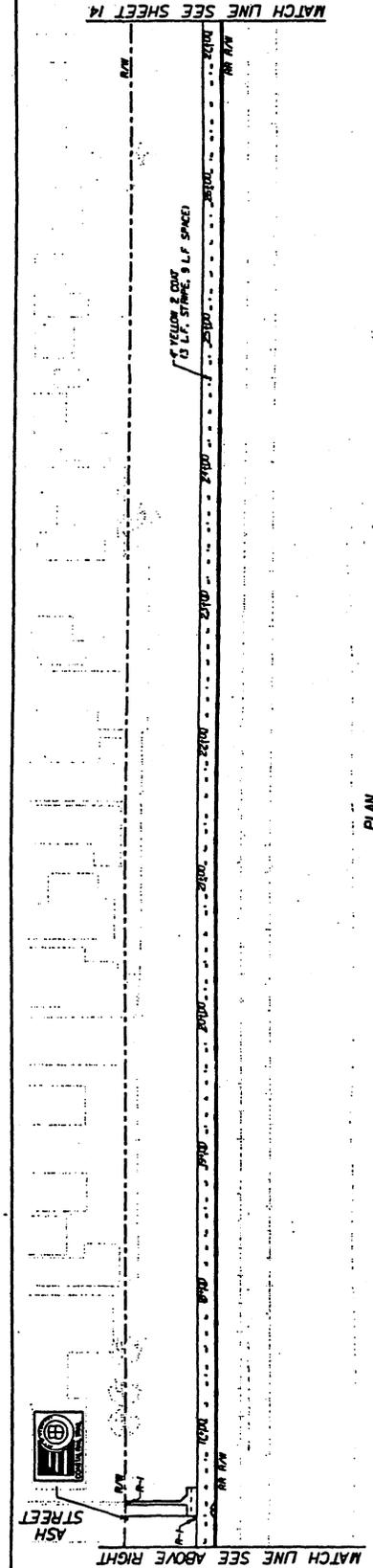
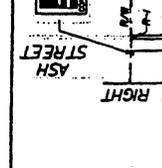
11111 OCEANSIDE BLVD
OCEANSIDE, CA 92057
TEL: (760) 434-1111
FAX: (760) 434-1112
WWW.DEDOKKEN.COM



MATCH LINE SEE SHEET 12

MATCH LINE SEE BELOW LEFT

PLAN
SCALE 1/4\"/>



MATCH LINE SEE ABOVE RIGHT

MATCH LINE SEE SHEET 14

PLAN
SCALE 1/4\"/>

- LEGEND**
- PROPOSED PCC
 - EXISTING PCC
 - PROPOSED CRUSHED AGGREGATE BASE
 - STAMPED CONCRETE
 - EXISTING GAS
 - EXISTING ELECTRIC
 - EXISTING TELEPHONE
 - EXISTING SEWER
 - EXISTING FIBER OPTIC
 - EXISTING STORM DRAIN
 - STATION LINE
 - WELDED WIRE MESH FENCE PER CONSP DETAIL SH-11
 - DRAINAGE DITCH
 - CITY R/W

- NOTES**
1. FOR FINISH LIFT STAMPING SEE CONSTRUCTION DETAIL SHEET 16.
 2. ALL STAMPING SHALL BE SUBJECT TO LIST OF NEW PCC FOUNDATIONS. ALL SOFTS WITH NO SOFT CALL OUT REQUIRE SOFT FOUNDATION CONSTRUCTION.



FOR SUBMITTAL - NOT FOR CONSTRUCTION
OCTOBER 15, 2004

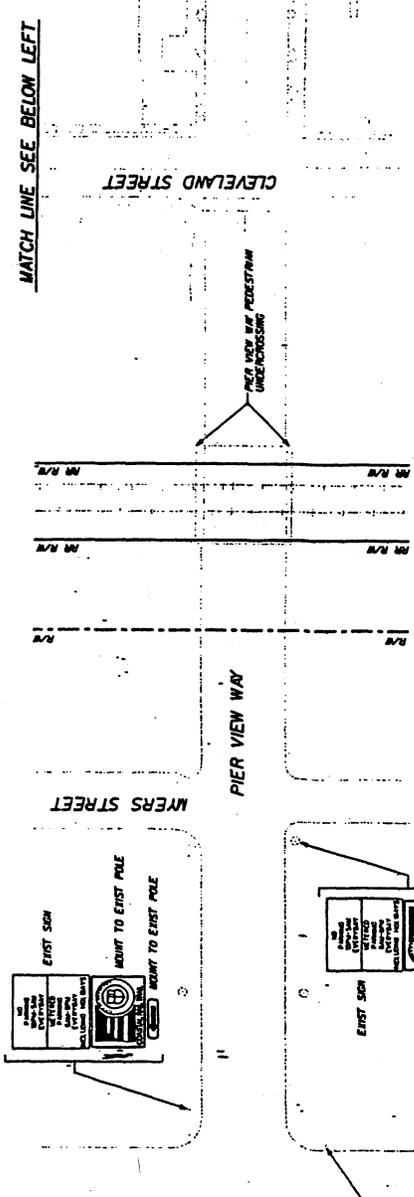
| | | | |
|-------------|---|------------------|----|
| DATE | 13 | CITY OF OCEANIDE | 34 |
| PROJECT | STAMPING AND SCHEDULE PLANS FOR COASTAL BAY TRAIL - PHASE 2 | | |
| DESIGNER | CLASS 1 TRAIL | | |
| CHECKED | E.C. JONES | | |
| DATE | 10/15/04 | | |
| PROJECT NO. | R-333333 | | |



D. DOKKEN
REGISTERED PROFESSIONAL ENGINEER
No. 12345
State of Oregon
1111 Main St., Suite 100
Corvallis, OR 97331
Tel: 541-338-1234

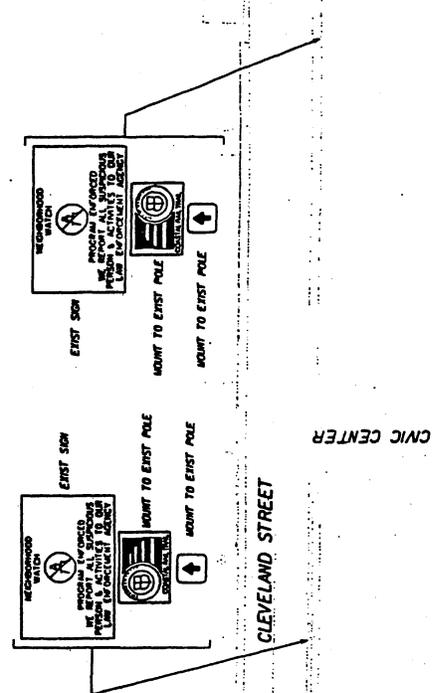
| NO. | REVISION | DATE |
|-----|----------|------|
| | | |
| | | |
| | | |

MATCH LINE SEE BELOW LEFT



PLAN
SHEET 7-B

MATCH LINE SEE SHEET 16



| | | | |
|------|----|-------------------------------|------------|
| DATE | 15 | CITY OF OCEANBORO | 31 |
| | | DESIGNED BY | 31 |
| | | STRIKING AND SEWAGE PLANS FOR | |
| | | COASTAL RAIL TRAIL - PHASE 2 | |
| | | CLASS ROUTE | |
| | | PROJECT NO. | 00000 |
| | | DATE OF PLAN | 08.11.2004 |
| | | PROJECT NAME | P-111111 |
| | | DRAWN BY | 00000 |
| | | CHECKED BY | 00000 |

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OCTOBER 15, 2004

PLAN
SHEET 7-B

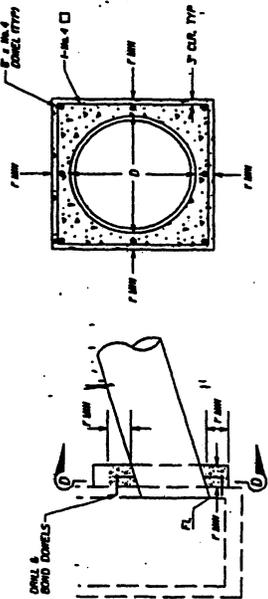
NOTES
1. ALL SPOCS SHOW AS "MOUNT TO EXIST. POLE" UNLESS OTHERWISE NOTED. ALL SPOCS SHALL BE MOUNTED TO EXISTING POLES UNLESS OTHERWISE NOTED. ALL SPOCS SHALL BE MOUNTED TO EXISTING POLES UNLESS OTHERWISE NOTED.

MATCH LINE SEE SHEET 14

LEGEND

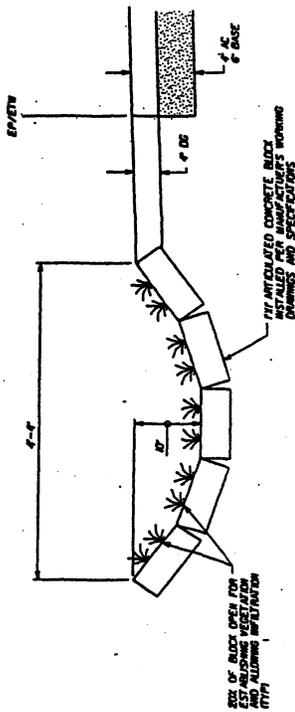
| | |
|----------|--|
| [Symbol] | PROPOSED COMPACTED FILL |
| [Symbol] | EXISTING ELECTRIC |
| [Symbol] | EXISTING WATER |
| [Symbol] | EXISTING TELEPHONE |
| [Symbol] | EXISTING SEWER |
| [Symbol] | EXISTING SPOCS OPTIC |
| [Symbol] | EXISTING SPOCS DRAIN |
| [Symbol] | STATION LINE |
| [Symbol] | 10'-0" WELDED WIRE MESH FENCE PER CONST DETAIL SHIT 17 |
| [Symbol] | DRAINAGE DITCH |
| [Symbol] | RR R/W |
| [Symbol] | CITY R/W |

DE DOKKEN ENGINEERING
INCORPORATED
100 Ocean Blvd, Suite 100
Ocean, NJ 07061
Tel: 908.261.1111
Fax: 908.261.1112
www.dedokken.com

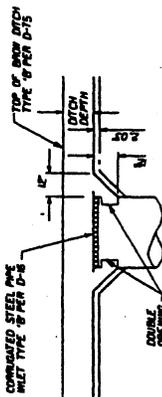


SECTION D-D
NO SCALE

CONNECTION OF PIPE TO EXISTING INLET
NO SCALE

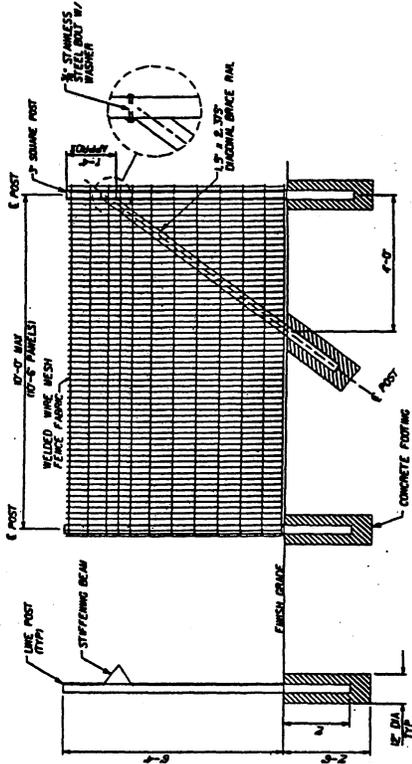


LINED DITCH WITH ARTICULATED CONCRETE BLOCK
NO SCALE



SECTION F-F
NO SCALE

CSP INLET TYPE 'B' (MOD.) WITH CONCRETE APRON
NO SCALE



WELDED WIRE MESH FENCE
NO SCALE

70% SUBMITTAL - NOT FOR CONSTRUCTION
OCTOBER 15, 2008

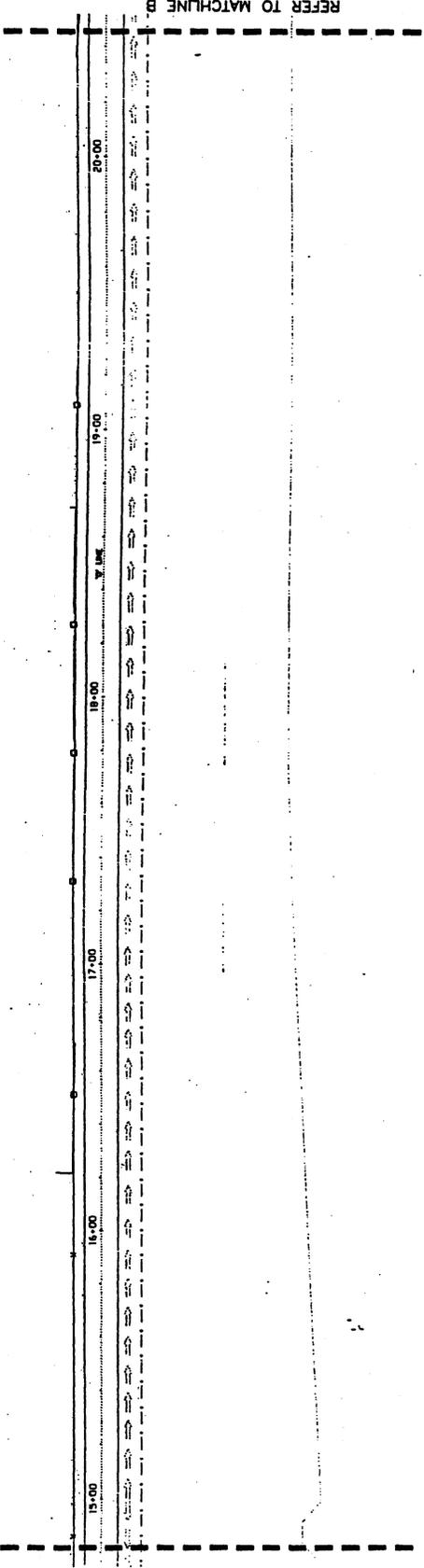
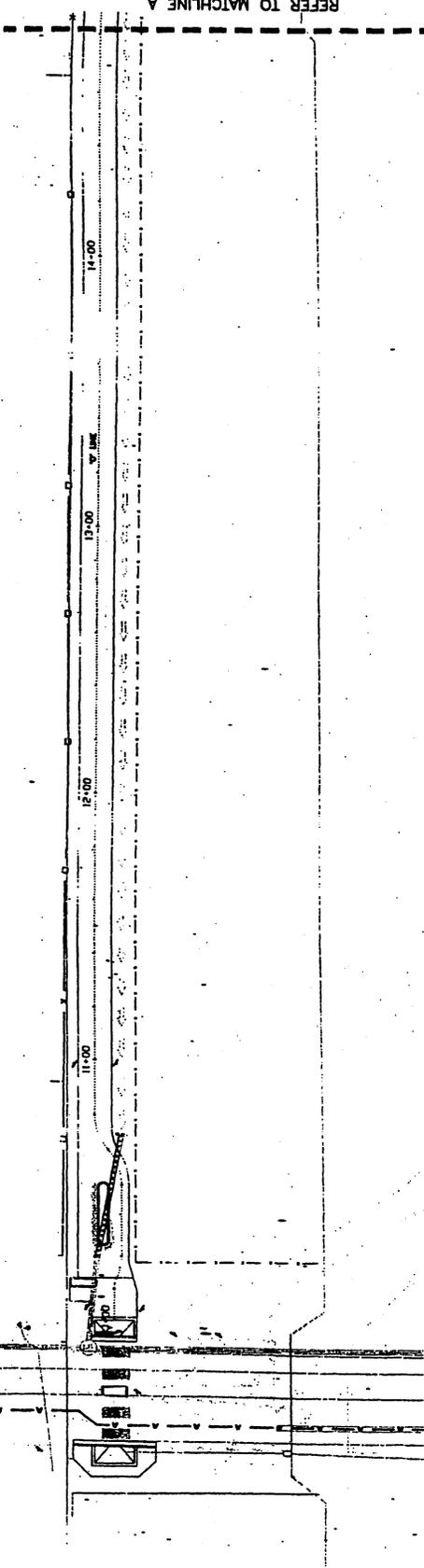
| | | |
|-------------|--|------------|
| DATE | CITY OF OCEANVIEW ENGINEERING DEPARTMENT | 34 |
| PROJECT NO. | CONSTRUCTION DETAILS COASTAL RAIL TRAIL - PHASE 2 | |
| DESIGNER | DESIGNED BY | CHECKED BY |
| DATE | DATE | DATE |

DEBOKKEN
ENGINEERS & ARCHITECTS
INC.

1000 W. BROADWAY
SUITE 200
OCEANVIEW, DE 19154

PH: 302.438.1100
FAX: 302.438.1101
WWW.DEBOKKEN.COM





| | |
|------------------------------|-------------|
| DATE | 10-15-04 |
| CITY OF OCEANSIDE | PROJECT NO. |
| 21 | 10-15-04 |
| IRREGULAR PLAN FOR: | |
| COASTAL PAL TRAIL - PHASE 2 | |
| DESIGNED BY | DATE |
| WALLACE ROBERTS & TODD, INC. | 10-15-04 |
| CHECKED BY | DATE |
| WALLACE ROBERTS & TODD, INC. | 10-15-04 |
| APPROVED BY | DATE |
| WALLACE ROBERTS & TODD, INC. | 10-15-04 |

70% SUBMITTAL - NOT FOR CONSTRUCTION

10-15-04

PROJECT NAME

COASTAL PAL TRAIL - PHASE 2

DATE

10-15-04

SCALE

1" = 20'-0"

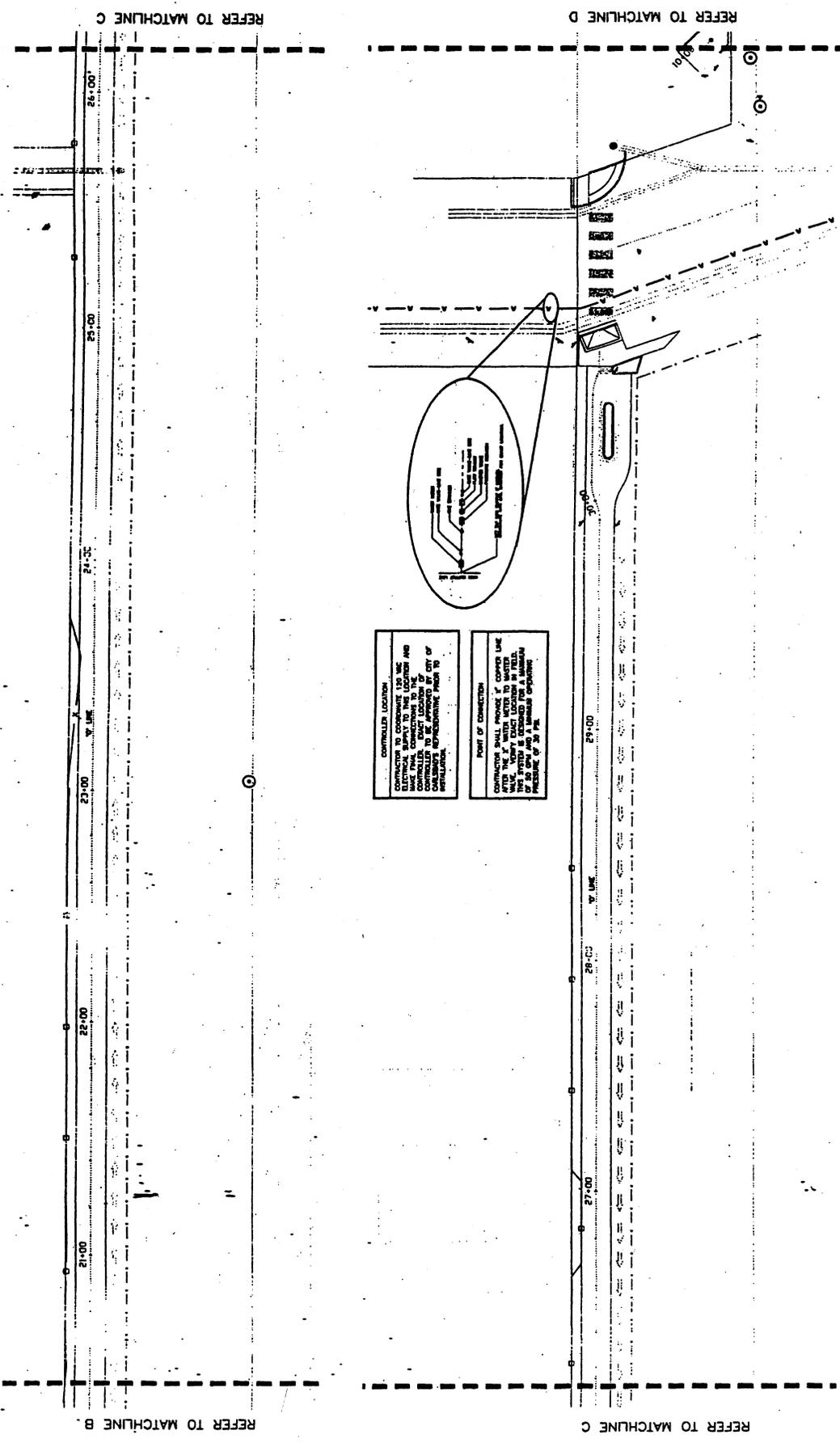
| NO. | DESCRIPTION | DATE | BY |
|-----|-------------|------|----|
| | | | |
| | | | |
| | | | |
| | | | |



Wallace Roberts & Todd, Inc.
 1117 Columbia Street, Suite 205
 San Diego, CA 92101
 Tel: (619) 444-7700
 Fax: (619) 444-7705



 SCALE: 1" = 20'-0"



CONTROLLER LOCATION
 CONTRACTOR TO PROVIDE 120V AC ELECTRICAL SUPPLY TO THE LOCATION AND WIRING TO THE CONTROLLER. THE CONTROLLER TO BE APPROVED BY CITY OF OAKLAND. CONTRACTOR TO PROVIDE PUNCH TO INSTALLATION.

POINT OF CONNECTION
 CONTRACTOR SHALL PROVIDE 1" COPPER LINE WITH THE 1" WATER METER TO WATER MAIN. THIS SYSTEM IS DESIGNED FOR A MINIMUM PRESSURE OF 30 PSI.

10-15-04
 70% SUBMITTAL - NOT FOR CONSTRUCTION

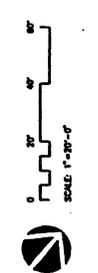
CITY OF OAKLAND
 ENGINEERING PLAN FOR:
COASTAL RAIL TRAIL - PHASE 2

DATE: 10/15/04
 DRAWN BY: J.L. BROWN
 CHECKED BY: J.L. BROWN
 PROJECT NO.: 04000008
 U.S.A. - 3111 E.P. 02/05

NOTE: THE CONTRACTOR'S OFFICE IS ADVISED TO BE AWARE OF THE CITY OF OAKLAND'S REQUIREMENTS FOR THE SUBMITTAL OF THIS PLAN. THE CITY OF OAKLAND'S REQUIREMENTS FOR THE SUBMITTAL OF THIS PLAN ARE AS FOLLOWS:
 1. ALL SUBMITTALS MUST BE MADE TO THE CITY ENGINEER'S OFFICE.
 2. ALL SUBMITTALS MUST BE MADE TO THE CITY ENGINEER'S OFFICE.
 3. ALL SUBMITTALS MUST BE MADE TO THE CITY ENGINEER'S OFFICE.

| NO. | CITY APPROVED CHANGE | DATE | BY |
|-----|----------------------|------|----|
| | | | |
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Wellies Roberts & Todd, Inc.
 1330 California Street, Suite 200
 San Diego, CA 92101
 Tel: 619.584.7275



REFER TO MATCHLINE G

REFER TO MATCHLINE F

REFER TO MATCHLINE G

| | | |
|---|--|--------------------------|
| Sheet No. 24 | CITY OF OCEANVIEW PLANNING DEPARTMENT | Scale 1" = 20' |
| PROVISION PLAN FOR COASTAL RAIL TRAIL - PHASE 2 | | |
| DATE: 11.12.2003 | PROJECT NO. 2003-003 | DESIGNED BY: JEFF HARRIS |
| CHECKED BY: JEFF HARRIS | DATE: 11.12.2003 | PROJECT NO. 2003-003 |
| S.A. 101.12.2003 | | |

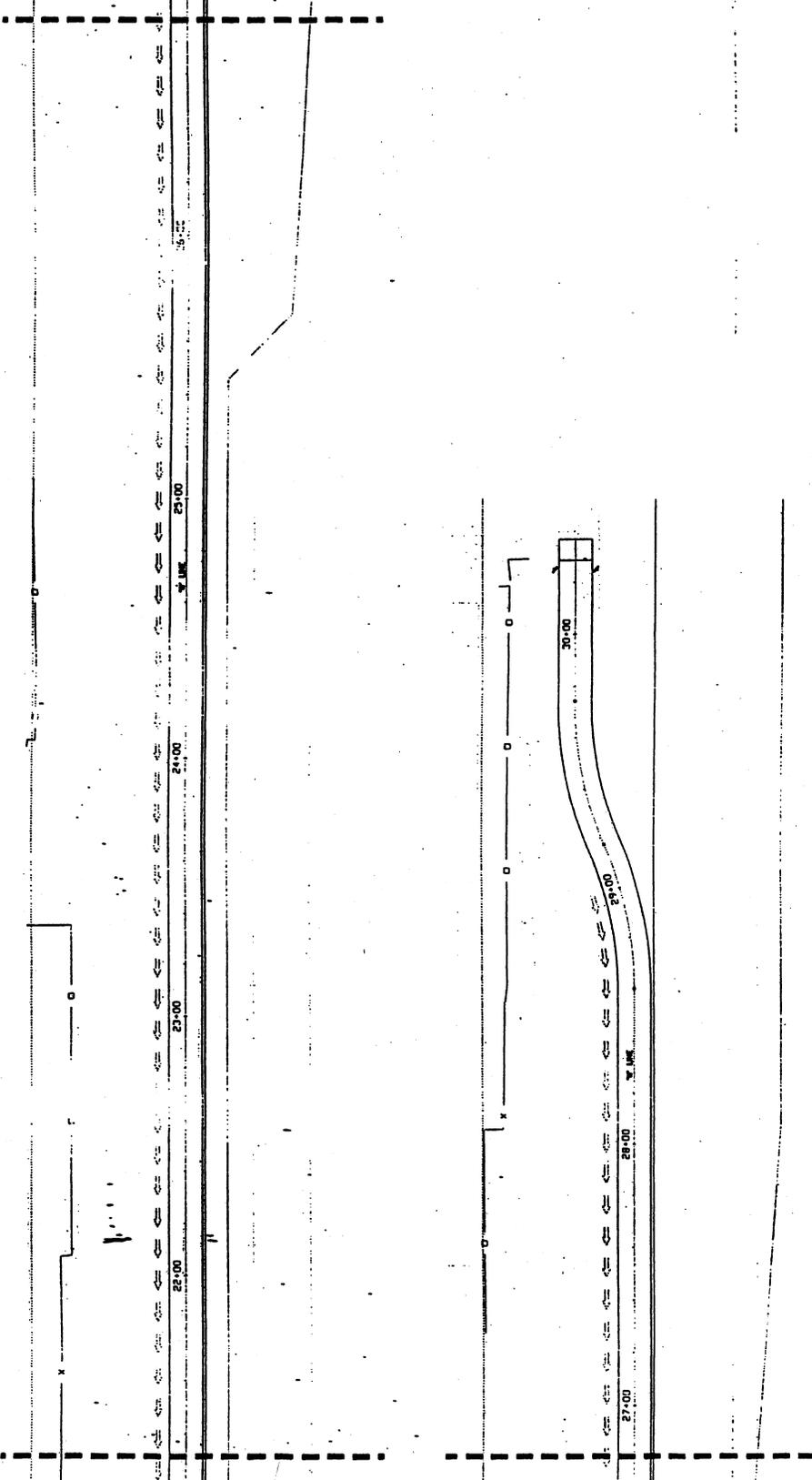
10-15-04
70% SUBMITTAL - NOT FOR CONSTRUCTION

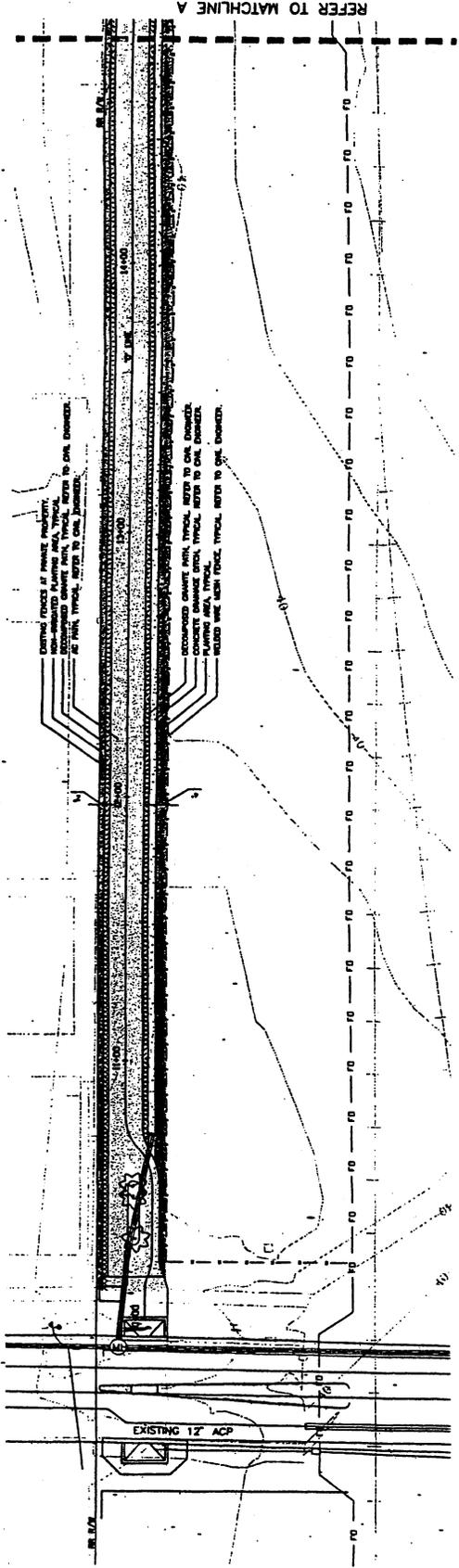
NOTICE: THE CONTRACTOR'S ATTENTION IS DRAWN TO THE FACT THAT THE CITY OF OCEANVIEW IS NOT RESPONSIBLE FOR THE ACCURACY OF THE INFORMATION CONTAINED IN THIS PLAN. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING ALL INFORMATION CONTAINED HEREIN.

DESIGNED BY: JEFF HARRIS
CHECKED BY: JEFF HARRIS
DATE: 11.12.2003
PROJECT NO. 2003-003

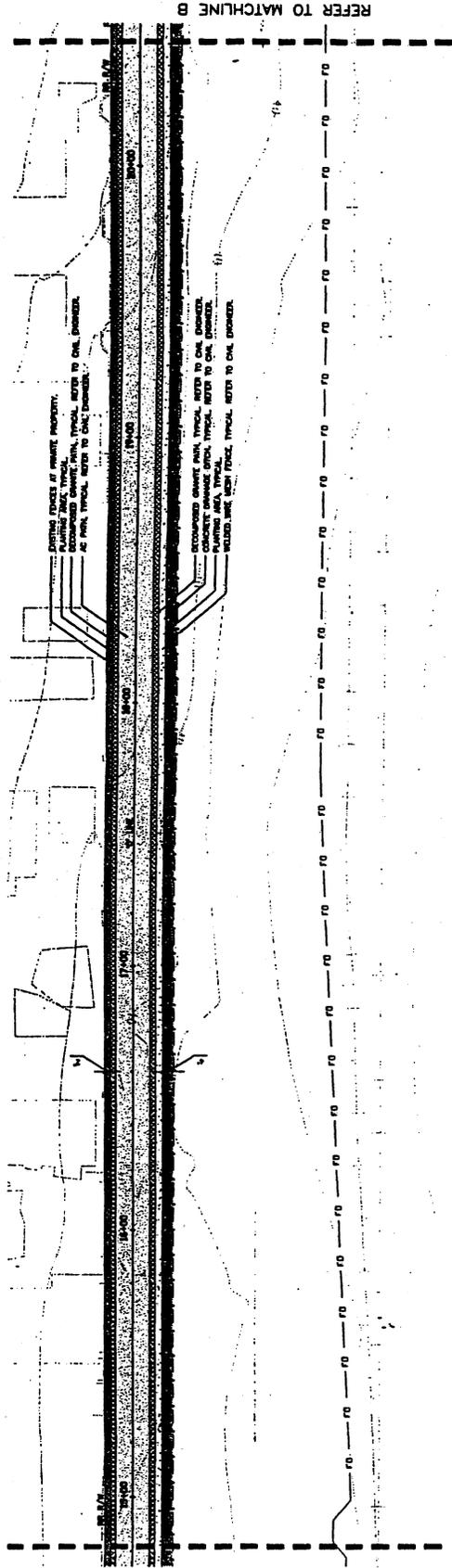
| NO. | DESCRIPTION | DATE | BY |
|-----|-------------|------|----|
| | | | |
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Wallace Roberts & Todd, Inc.
113 Columbia Street, Suite 205
San Diego, CA 92101
Tel: 619.494.1700
Fax: 619.494.1702





REFER TO MATCHLINE A



REFER TO MATCHLINE B

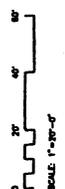
10-15-04
 70% SUBMITTAL - NOT FOR CONSTRUCTION

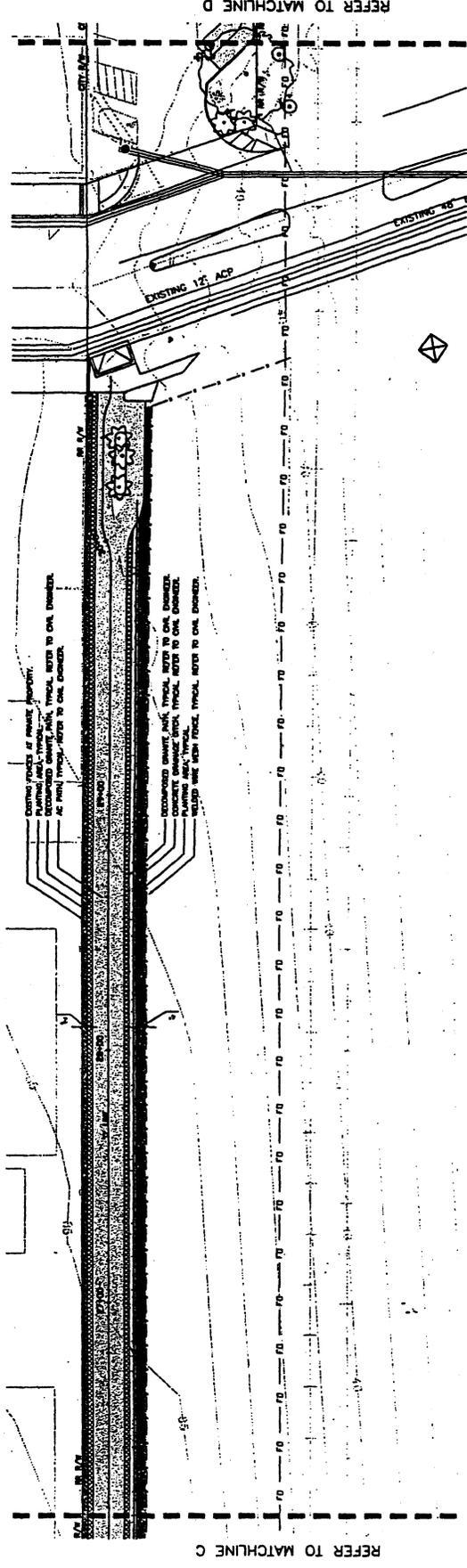
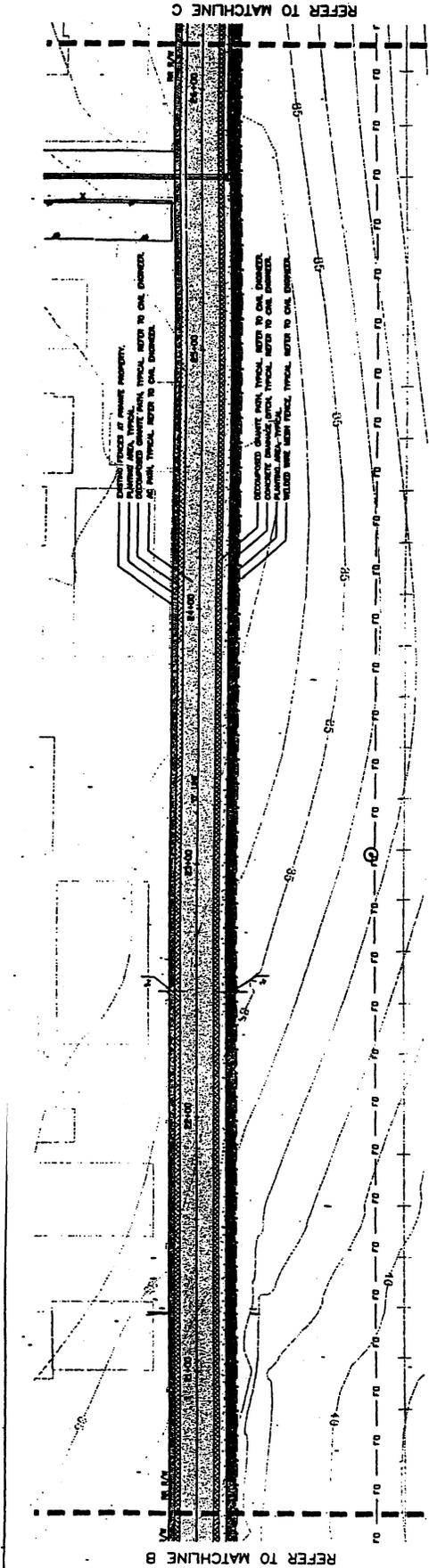
| | | | |
|-------------|------------------------------|--------------------|------------------------------|
| DATE | 28 | CITY OF OGDENHURST | 37 |
| PROJECT NO. | 10-15-04 | PROJECT NAME | COASTAL RAIL TRAIL - PHASE 2 |
| DESIGNED BY | WALLACE ROBERTS & TODD, INC. | DATE | 10/15/04 |
| CHECKED BY | WALLACE ROBERTS & TODD, INC. | SCALE | AS SHOWN |
| DATE | 10/15/04 | PROJECT NO. | 10-15-04 |

NOTE: THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UTILITIES AND STRUCTURES TO REMAIN. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UTILITIES AND STRUCTURES TO REMAIN. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UTILITIES AND STRUCTURES TO REMAIN.

| NO. | DESCRIPTION | DATE |
|-----|-------------|------|
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| | | |

Wallace Roberts & Todd, Inc.
 1121 Colorado Street, Suite 201
 San Diego, CA 92101
 Tel: 619.444.7375





10-15-04
 70% SUBMITTAL - NOT FOR CONSTRUCTION

CITY OF BEAUMONT
 ENGINEER

PLANNING PLAN FOR
COASTAL RAIL TRAIL - PHASE 2

DATE: 10-15-04
 DRAWN BY: [Name]
 CHECKED BY: [Name]
 SCALE: [Scale]

10-15-04
 70% SUBMITTAL - NOT FOR CONSTRUCTION

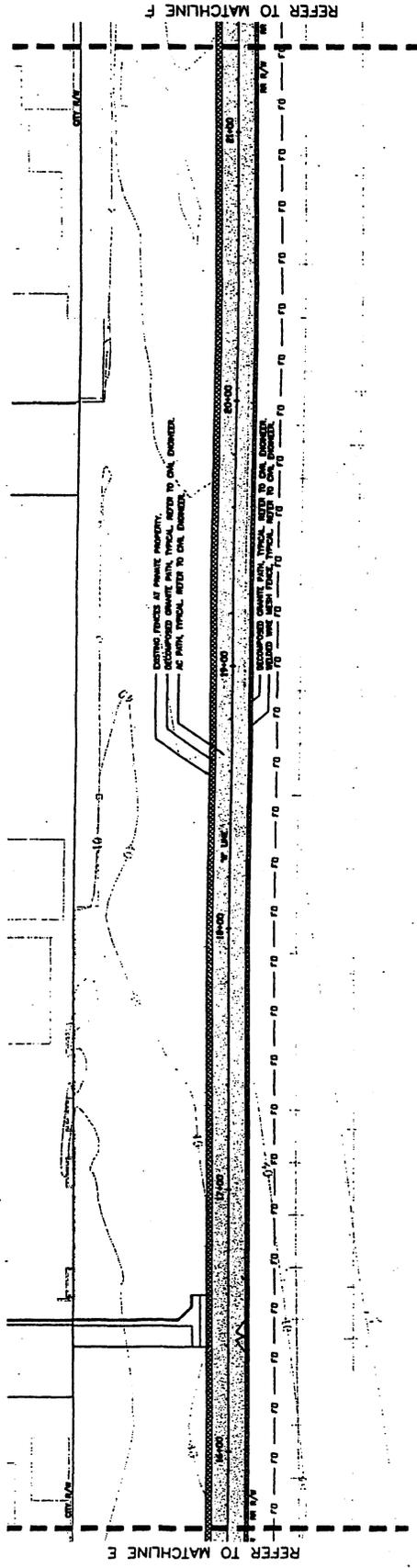
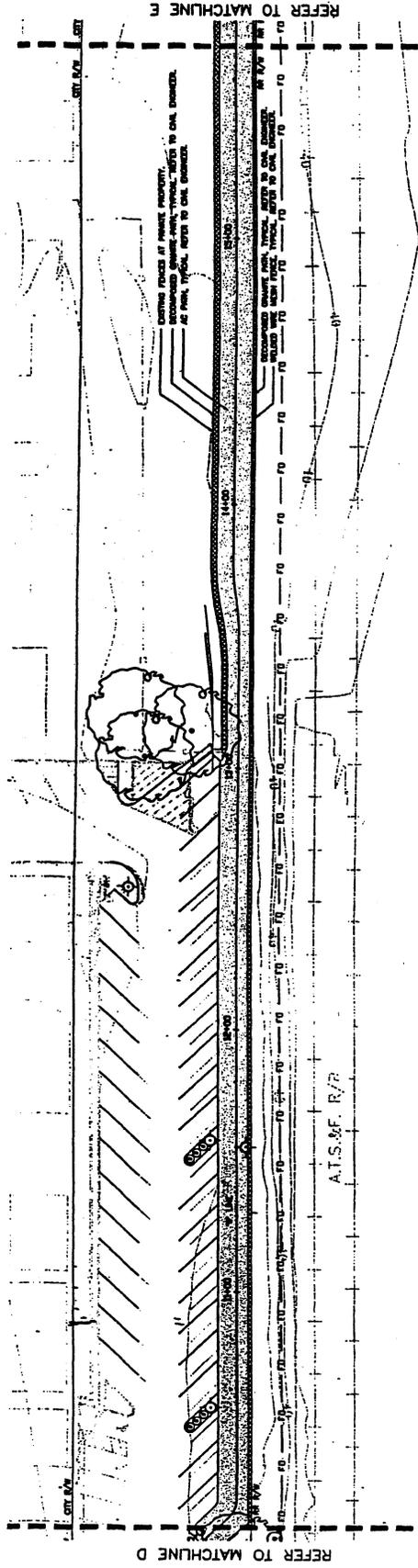
WALLACE ROBERTS & TODD, INC.
 1121 CALLEJA STREET, SUITE 205
 SAN ANGELO, TEXAS 76901
 TEL: 361-843-7777

WALLACE ROBERTS & TODD, INC.
 1121 CALLEJA STREET, SUITE 205
 SAN ANGELO, TEXAS 76901
 TEL: 361-843-7777

10-15-04
 70% SUBMITTAL - NOT FOR CONSTRUCTION

WALLACE ROBERTS & TODD, INC.
 1121 CALLEJA STREET, SUITE 205
 SAN ANGELO, TEXAS 76901
 TEL: 361-843-7777

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 1121 CALLEJA STREET, SUITE 205
 SAN ANGELO, TEXAS 76901
 TEL: 361-843-7777



10-15-04

70% SUBMITTAL - NOT FOR CONSTRUCTION

CITY OF OCEANVIEW
30

PLANNING PLAN FOR:
COASTAL TRAIL TRAIL - PHASE 2

DATE: 10-15-04

PROJECT NO: 10-15-04

DRAWN BY: [Name]

CHECKED BY: [Name]

SCALE: 1"=30'-0"

70% SUBMITTAL - NOT FOR CONSTRUCTION

10-15-04

DATE: 10-15-04

PROJECT NO: 10-15-04

DRAWN BY: [Name]

CHECKED BY: [Name]

SCALE: 1"=30'-0"

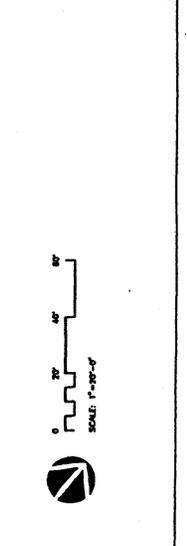
| NO. | DESCRIPTION | DATE | BY |
|-----|-------------|------|----|
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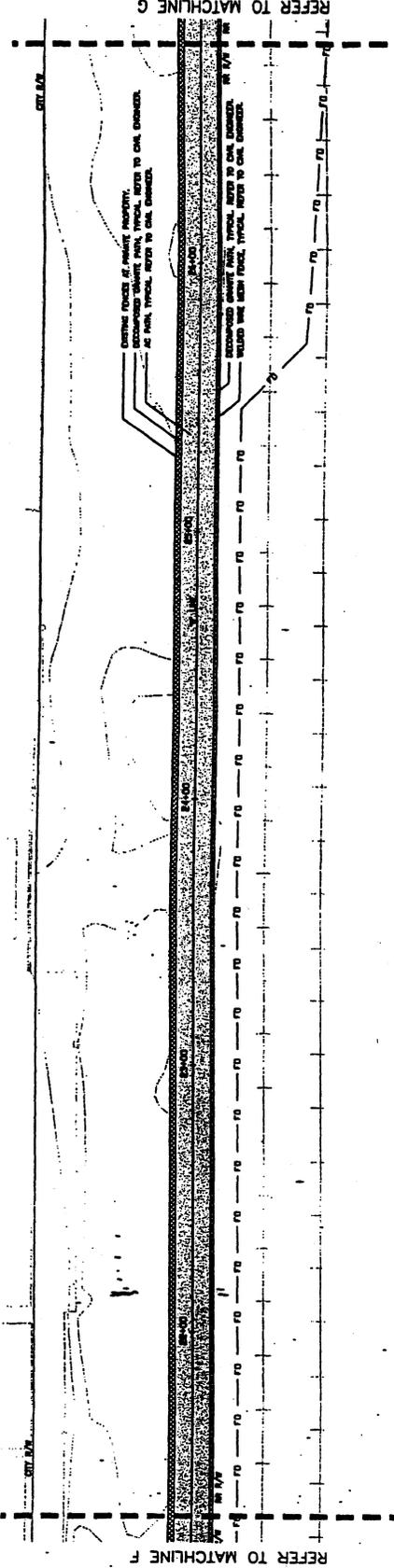
Wallace Roberts & Todd, Inc.
1133 Columbia Street, Suite 205
San Diego, CA 92101
Tel: 619.494.1725

APPROVED FOR CONSTRUCTION

DATE: 10-15-04

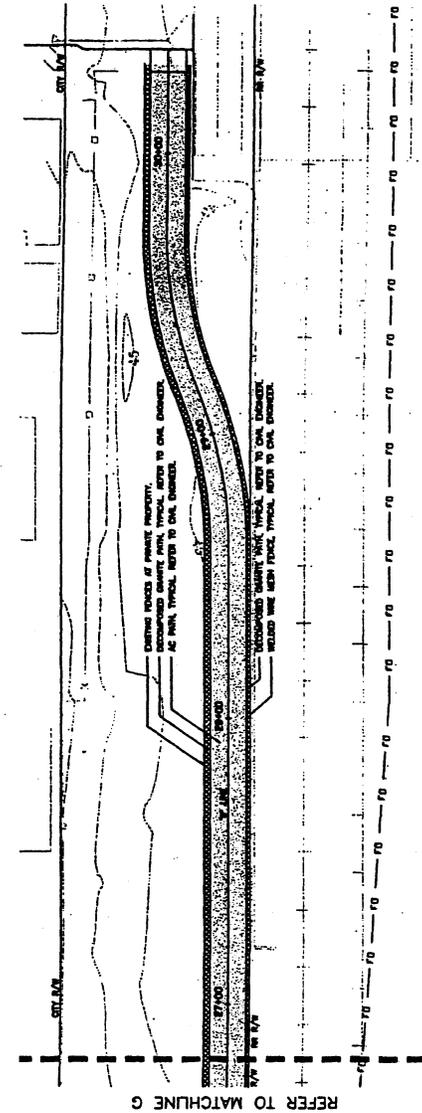
PROJECT NO: 10-15-04





REFER TO MATCHLINE F

REFER TO MATCHLINE G



REFER TO MATCHLINE G



10-15-04
70% SUBMITTAL - NOT FOR CONSTRUCTION

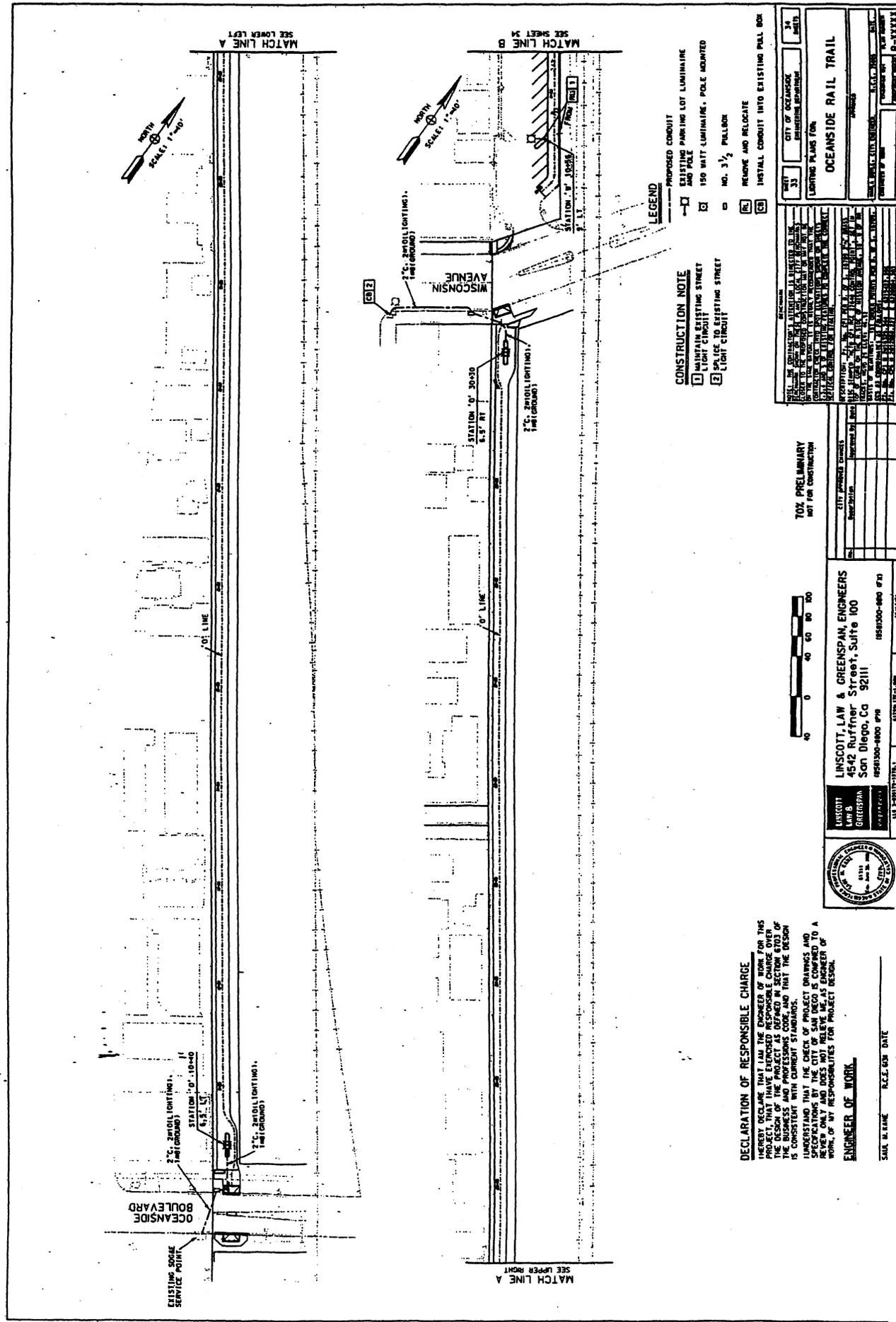
SHEET 31
CITY OF OCEANSIDE
PLANNING PLAN FOR:
COASTAL TRAIL - PHASE 2

NOTES: THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE CITY OF OCEANSIDE'S STANDARD SPECIFICATIONS FOR ROADWAY CONSTRUCTION AND TO THE CITY OF OCEANSIDE'S STANDARD SPECIFICATIONS FOR LANDSCAPE CONSTRUCTION.
1. THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL ADJACENT PROPERTIES AT ALL TIMES.
2. THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL ADJACENT UTILITIES AT ALL TIMES.
3. THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL ADJACENT UTILITIES AT ALL TIMES.
4. THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL ADJACENT UTILITIES AT ALL TIMES.
5. THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL ADJACENT UTILITIES AT ALL TIMES.
6. THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL ADJACENT UTILITIES AT ALL TIMES.
7. THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL ADJACENT UTILITIES AT ALL TIMES.
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9. THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL ADJACENT UTILITIES AT ALL TIMES.
10. THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL ADJACENT UTILITIES AT ALL TIMES.

DATE: 10-15-04
DRAWN BY: [Name]
CHECKED BY: [Name]
APPROVED BY: [Name]
SCALE: 1\"/>



Wallace Roberts & Todd, Inc.
1133 Callesandar Street, Suite 205
San Diego, CA 92101
Tel: 619.444.7752



CONSTRUCTION NOTE

- 1) LIGHTING EXISTING STREET
- 2) SPACES TO EXISTING STREET LIGHT CIRCUIT

LEGEND

- PROPOSED CONDUIT
- EXISTING PARKING LOT LUMINAIRE AND POLE
- 150 WATT LUMINAIRE, POLE MOUNTED
- NO. 3 1/2 PULLBOX
- REMOVE AND RELOCATE
- INSTALL CONDUIT INTO EXISTING PULL BOX

DECLARATION OF RESPONSIBLE CHARGE

HEREBY DECLARE THAT I AM THE ENGINEER OF WORK FOR THIS PROJECT, THAT I HAVE EXERCISED RESPONSIBLE CHARGE OVER THE DESIGN OF THE PROJECT AS DEFINED IN SECTION 6703 OF THE PUBLIC WORKS ACT AND THAT THE DESIGN IS CONSISTENT WITH CURRENT STANDARDS.

I UNDERSTAND THAT THE CHECK OF PROJECT DRAWINGS AND SPECIFICATIONS BY THE CITY OF SAN DIEGO IS CONFINED TO A REVIEW ONLY AND DOES NOT RELIEVE ME AS ENGINEER OF WORK OF MY RESPONSIBILITIES FOR PROJECT DESIGN.

ENGINEER OF WORK

SAVA, M. SAHE R.C.E. SIGN DATE



70% PRELIMINARY
NOT FOR CONSTRUCTION

LINSCOTT LAW & GREENSPAN ENGINEERS
4542 Ruffner Street, Suite 100
San Diego, Ca 92111
619-593-0000 #70
619-593-0000 #70



| | |
|-----------------------------|-----------------|
| PROJECT NO. | 33 |
| CITY OF OCEANSIDE | PROJECT NO. |
| LIGHTING PLANS FOR | |
| OCEANSIDE RAIL TRAIL | |
| DATE: 03/11/2010 | SCALE: AS SHOWN |
| PROJECT NO. | 33 |
| CITY OF OCEANSIDE | PROJECT NO. |
| LIGHTING PLANS FOR | |
| OCEANSIDE RAIL TRAIL | |
| DATE: 03/11/2010 | SCALE: AS SHOWN |

